



BRADY VILLAGE INFILL DEVELOPMENT DESIGN RECOMMENDATIONS

A COMPONENT OF THE BRADY VILLAGE INFILL NEIGHBORHOOD
DETAILED IMPLEMENTATION PLAN

Prepared by the:

Brady Village Infill Task Force

In cooperation with:

Planning and Economic Development Division
Urban Development Department
City of Tulsa

December 17, 2002

BRADY VILLAGE INFILL DEVELOPMENT DESIGN RECOMMENDATIONS

A COMPONENT OF THE BRADY
VILLAGE INFILL
NEIGHBORHOOD
DETAILED IMPLEMENTATION
PLAN

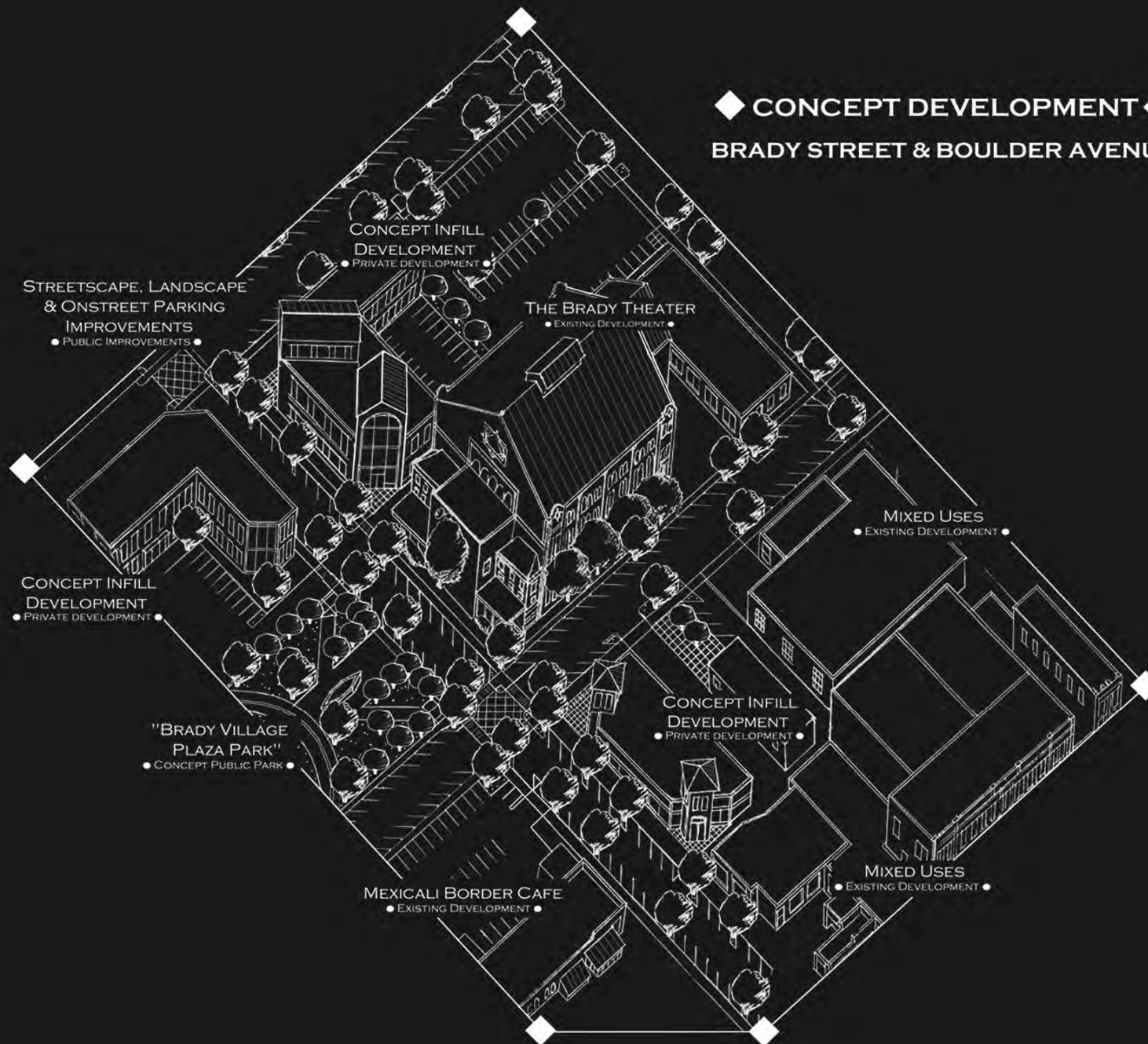
Prepared by the:

**Brady Village
Infill Task Force**

In cooperation with:

Planning and Economic
Development Division
Urban Development Department

◆ **CONCEPT DEVELOPMENT** ◆
BRADY STREET & BOULDER AVENUE



December 17, 2002

TABLE OF CONTENTS

<u>ITEM</u>	<u>PAGE</u>
TABLE OF CONTENTS	i
TABLE OF EXHIBITS	iv
LIST OF APPENDICES	vi
PREFACE AND PLANNING FRAMEWORK	vii
PURPOSE	1
INTRODUCTION	1
OVERVIEW	2
THE BRADY VILLAGE AREA	5
1. Overall Design Policies	5
A. General	5
B. Land Use	5
C. Transportation	6
D. Parking	7
E. Urban Design	7
F. Location of Key Design Elements	12

2.	Specific Design Policies	13
A.	Street Furniture	13
(1)	Benches	14
(2)	Trash Receptacles	15
(3)	Bicycle Racks	16
(4)	Bollards	18
B.	Paving Materials	19
(1)	Street Paving	19
(2)	Pedestrian Crosswalks	19
(3)	Sidewalks	20
(4)	Sidewalk Business Entries	21
(5)	“Bump-Outs” or Curb Extensions	22
C.	Street Trees and Plant Materials	23
(1)	In-Ground Tree Planters (Tree Pits)	24
(2)	Tree Grates	25
D.	Lighting	26

(1)	Pedestrian Lights	26
(2)	Roadway Lights	28
(3)	Identity-Locator-Pinnacle (ILP) Light System	29
(4)	Building Facade Lights	31
E.	Signs and Graphic Art	31
F.	Public Art and Monuments	31
3.	Brady Village Concept Development Plan	32

TABLE OF EXHIBITS

<u>EXHIBIT</u>	<u>PAGE</u>
EXHIBIT 1 – “Brady Village Location Map”	x
EXHIBIT 2 – “T.I.F. District No. 1 and Study Area - Brady Village”	3
EXHIBIT 3 – “Brady Village T.I.F. District No. 1 and Study Area (Aerial)	4
EXHIBIT 4 – “Brady Village Infill Development•	9
EXHIBIT 5 – “Location of Key Design Elements - Brady Village•	12
EXHIBIT 6 – “Typical Decorative Bench•	14
EXHIBIT 7 – “Typical Trash Receptacle•	15
EXHIBIT 8 – “Typical Bicycle Racks (Option 1)•	16
EXHIBIT 9 – “Typical Bicycle Racks (Option 2)•	17
EXHIBIT 10 – “Typical Bollard•	18
EXHIBIT 11 – “Typical Textured Pavement Patterns For Sidewalks, Parking Lots and Streets•	20
EXHIBIT 12 – “Typical Textured Surface or Pavement Treatment for Business Entries•	21
EXHIBIT 13 – “Typical ‘Bump-outs’ or Curb Extensions” and “Typical Concept Design For Crosswalks and Street Intersection Improvements”	22
EXHIBIT 14 – “Typical Tree Pit and Tree Grate Installation”	24

EXHIBIT 15 – “Typical Tree Grates”	25
EXHIBIT 16 – “Typical Ornamental Lamp Post (Pedestrian)”	26
EXHIBIT 17 – “Typical Collars and Finials For Ornamental Lamp Post (Pedestrian)”	27
EXHIBIT 18 – “Typical Example of Ornamental Pedestrian Lamp Globe-Fixture”	27
EXHIBIT 19 – “Lamp and Light Fixture Close-up”	28
EXHIBIT 20 – “Lamp, Light Fixture and Pole”	28
EXHIBIT 21 - Concept “Identity-Locator-Pinnacle (ILP) Light System Tower and Archways”	29
EXHIBIT 22 - “South Perspective – ‘ILP’ Lighting System Arch and BOK Parking Lot Arch - Boston Avenue Pedestrian Bridge”	30
EXHIBIT 23 – “Index Map of Concept Development Plan Venues”	33
EXHIBIT 24 – “Concept Development Plan Venue - Southeast Quadrant”	34
EXHIBIT 25 – “Concept Development Plan Venue - Northeast Quadrant”	35
EXHIBIT 26 – “Concept Development Plan Venue - Southwest Quadrant”	36
EXHIBIT 27 – “Concept Development Plan Venue - Northwest Quadrant”	37

LIST OF APPENDICES:

APPENDIX A – “RECOMMENDED LIST OF PHYSICAL/CAPITAL IMPROVEMENT PROJECTS OF THE NEIGHBORHOOD TASK FORCE”, September 2000

APPENDIX B - PROPOSED 2001-2006 THIRD PENNY SALES TAX EXTENSION CAPITAL IMPROVEMENT PROGRAM PROJECTS FOR THE AREA, Extension Approved May 2001

APPENDIX C – “THE DISTRICT 1 PLAN MAP”

APPENDIX D – “THE DISTRICT 1 PLAN MAP- BRADY VILLAGE AREA”

APPENDIX E – “RECENT PHOTOGRAPHS OF BRADY VILLAGE AREA”

APPENDIX F - BRADY VILLAGE BICYCLE STREET ROUTE AND TRAIL PLAN - WEST PORTION and BRADY VILLAGE BICYCLE STREET ROUTE AND TRAIL PLAN - EAST PORTION

APPENDIX G - ARCHER STREET PARK ELEMENTS - CONCEPTS A, B, C AND D

APPENDIX H - EXAMPLES OF INFILL DEVELOPMENT INCENTIVES AND PROJECTS

APPENDIX I- EXAMPLES OF STREET FURNITURE AND LANDSCAPING

APPENDIX J - EXAMPLES OF “BUMP-OUTS”, CURB EXTENSIONS AND/OR CROSSWALKS

APPENDIX K - ACKNOWLEDGMENTS

APPENDIX L - CIRCULATION STUDY OF CINCINNATI AND DETROIT AVENUES BETWEEN THE I.D.L. AND ARCHER STREETS, BRADY VILLAGE

APPENDIX M - EXAMPLES OF APPROPRIATE MIXED USE PARKING FACILITIES

APPENDIX N - EXISTING ROADWAY LIGHTING AND PRELIMINARY ROADWAY LIGHTING PLAN

PREFACE AND PLANNING FRAMEWORK

Tulsa has reached its one hundredth birthday and, with respect to American urban places, Tulsa is a young child. Yet even in this youth, we are encountering what older urban centers have experienced for sometime. Tulsa has already reached in many places the physical limits to where our community can grow. Land for development has been greatly consumed and outward growth is blocked by surrounding suburban communities. As our community looks for new development opportunities, it must look inward at the older existing urban form. Tulsa leaders responded to this and other related urban issues by stating:

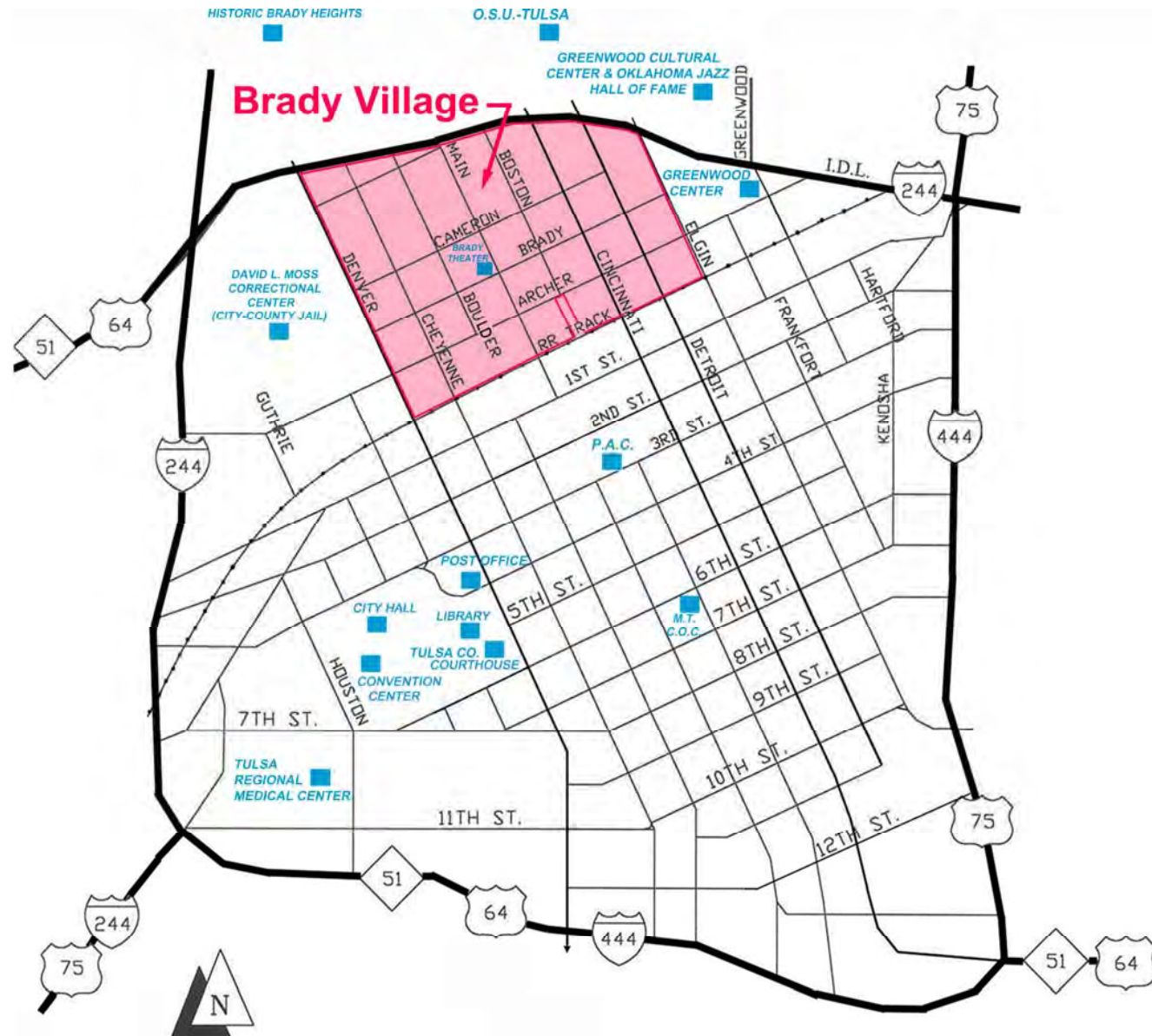
“As Tulsa continues to mature as a city, infill development will become more important as land on the perimeter is no longer available for development. Infill will no longer be the exception; it will be the rule in terms of predominant types of development. Support and encouragement of infill development are strongly recommended and should be implemented through City regulations, policies and philosophies in order to ensure quality and consistency in future development.”¹

This is the opening statement of the 1999 Tulsa report on infill development, “Report of the Infill Development Task Force,” prepared by the Tulsa Infill Development Task Force for the Mayor and the Tulsa Metropolitan Area Planning Commission. The Tulsa Infill Development Task Force recommendations are serving as the foundation for infill planning initiatives in the City of Tulsa.

This Task Force identified several major infill-development-related issues which needed to be addressed and suggested that a pilot study be conducted of a designated planning area which would result in the preparation of a detailed neighborhood plan for optimum development. Given the diversity of character and issues within Tulsa neighborhoods, the Mayor’s Office determined that three pilot studies would be undertaken by the City of Tulsa Urban Development Department to address infill issues. The Brady Village neighborhood area is one of those pilot study areas. The location of Brady Village in Downtown Tulsa is depicted in Exhibit 1 - Brady Village Location Map.

¹ “Report of the Infill Development Task Force”, May 11, 1999, Page 5.

EXHIBIT 1 -
BRADYVILLAGE
LOCATION MAP



The planning process for Brady Village began with the establishment of a Brady Village Infill Neighborhood Task Force (Brady Village Task Force). The Brady Village Task Force consisted initially of members of a previously established planning group for the Brady Village Tax Increment Finance (T.I.F.) District No. 1. During the course of the Infill Plan Study, additional members were added to the Task Force and the Brady Village property owners established the Brady Village Owners' Association to encourage and facilitate the physical and economic development in the area. Members included representatives from the Brady Village Owners' Association, area business and property owners, and individuals interested in the area's activities (refer to Appendix K for a list of Brady Village Task Force Members).

Many months were spent in determining community defined issues and concerns, formulating design considerations and opportunities, determining community preferred solutions and initiating near-term solutions to immediate problems, while targeting long-term strategies to "larger" issues. Brady Village needs were defined and an extensive survey was conducted to determine planning priorities. Study efforts led the Brady Village Task Force to identify specific infill issues which related directly or indirectly to the major points included in the citywide task force report. Those major points revolved around:

- (1) Determining recommendations for urban design solutions to help resolve land use and development conflicts;
- (2) Reaffirming existing and recommending new policies and standards related to infrastructure;
- (3) Determining new and upgraded public improvement needs and establishing priorities;
- (4) Seeking funding of new and upgraded public improvements;
- (5) Suggest revisions to planning, zoning and building codes;
- (6) Address immediate and long-term parking problems;
- (7) Improve traffic safety and circulation;
- (8) Provide area lighting for improved safety, security and identity;
- (9) Streamline the development permitting process;
- (10) Abate or reduce train traffic "whistles and horns"; and
- (11) Support crime prevention and security activities.

The Brady Village Task Force concluded that not any one strategy would solve an issue. Rather multiple strategies were deemed necessary and appropriate. It was also determined that solutions and proposals had differing time-frames in which they could and needed to be addressed. Some issues could be dealt within a rather short time-frame. These solutions involved strategies or actions that could begin almost immediately - near-term planning efforts. Other issues require more time to improve or solve, generally three to five years - midrange plan proposals. Still other recommendations were long-term in nature and would take longer to accomplish - long-range plan proposals. The Brady Village Task Force proceeded with a near-term planning effort targeting immediate needs.

Simultaneously, the Task Force began the development of the Brady Village Infill Neighborhood Detailed Implementation Plan for midrange and long-range proposals.

It is noted that infill planning efforts and activities by the City of Tulsa extend beyond the Urban Development Department and include significant efforts of the Department of Public Works and its several divisions. Additionally, infill planning coordination continues between the Tulsa Metropolitan Area Planning Commission, its staff and the Urban Development Department. Examples of infill planning efforts which have occurred and/or are ongoing in Brady Village or which directly relate to the Brady Village community and other pilot study areas are described below.

- (1) Determining recommendations for urban design solutions to help resolve land use and development conflicts and enhance development. The Task Force considered existing requirements for streetscaping and landscaping for Tulsa and reviewed examples of their application in Tulsa and Brady Village. Additional design elements which have been used in Downtown Tulsa and other communities were also considered and reviewed. Examples of their application in Tulsa were presented to the Task Force. Substantial discussion focused on these urban design features and the recommendations of the Brady Village Task Force are contained in this report document.
- (2) Reaffirming existing and recommending new policies and standards related to infrastructure. The District 1 Plan was reviewed generally by the Task Force as were other previous studies of Downtown Tulsa. A “design charrette” was held to consider these and other recommendations for Brady Village. Design standards are recommended for certain area public improvements, street paving, street furniture, tree planting and other streetscape features especially important to enhancing the image and beauty of the area. These standards are included within this Plan report and are intended to be adopted by the Tulsa Metropolitan Area Planning Commission and the City of Tulsa as they apply to Brady Village.
- (3) Determining new and upgraded public improvement needs and establishing priorities. The Brady Village Task Force identified capital improvement needs for the Brady Village area. The Task Force reviewed the projects, considered conceptual designs and determined a priority list of capital improvements. The “Recommended List of Physical/Capital Improvement Projects of the Brady Village Task Force” was submitted to the Tulsa Metropolitan Area Planning Commission and confirmed as conforming with the District 1 Plan by the Tulsa Metropolitan Area Planning Commission and their staff. That project list is contained in Appendix A of this Plan report.
- (4) Seeking funding of new and upgraded public improvements. Budgetary cost estimates were prepared for the priority projects and reviewed by the Task Force. These projects are included within this Plan report for inclusion in the District 1 Plan as it applies to Brady Village area. It was the expressed desire of the Brady Village Task Force that these projects be included in the Tulsa “Third Penny Sales Tax Extension” program of the City, as well as other possible funding programs of the City, state and federal government. Several of the projects were approved for funding by the 2001 Third Penny Sales Tax Extension and are included in a list of Brady Village area projects listed in Appendix B. The remaining projects have been submitted to the City for inclusion in the list of eligible capital projects for future capital improvements funding.

- (5) Suggesting revisions to planning, zoning and building codes. The problem of redeveloping and reusing the older existing building stock in Tulsa is common to the three pilot study areas, including Brady Village, and for all of the older urbanized portions of Tulsa. The City initiated a planning effort to review problems associated with application of the Tulsa (B.O.C.A.) Building Codes to infill projects in older buildings. This planning effort included lead staff from all City departments which deal with development and construction plans. The effort, led jointly by the Fire Marshall's Office and the Development Services Division of the Public Works Department, studied the existing building code, reviewed an "Existing Building Code" which had been adopted in New Jersey to facilitate infill development, and prepared a recommended "Existing Building Code" chapter for Tulsa's Building Code. Since this effort, the City of Tulsa has adopted a new version of an existing building code which can be used for older existing buildings. Application of this Code should remove many barriers to the economic, practical reuse of these buildings, while assisting in preservation of their architectural character and flavor. The use of this Code should facilitate the reuse and mixed use of existing buildings in Brady Village and elsewhere within older portions of Tulsa.
- (6) Addressing parking issues. This problem has been previously identified in Brady Village. Task Force efforts reaffirmed this as a major issue in the area. It is a particular problem when major construction efforts are underway in Downtown and area parking becomes a premium for office and construction workers. In the short-term, parking problems were experienced along Boston Avenue, Main Street, Boulder Avenue, Archer Street, Brady Street and Cameron Street. After careful study, it was determined that on-street angle-parking could be provided on streets having sufficient paving widths. All of Main Street and Boulder Street and portions of Brady Street and Cameron Street were striped to accommodate on-street angle-parking spaces. Budgetary cost estimates were prepared for conceptual parking projects and reviewed by the Task Force. Additional on-street parking solutions proposed by the Task Force are still under consideration and test projects have been constructed by property owners in the district. Other on-street and off-street public improvements were included in the "Recommended List of Physical/Capital Improvement Projects of the Brady Village Neighborhood Task Force" submitted to the City of Tulsa. The Plan proposes additional ongoing programming efforts necessary to develop the required consensus of area property owners, develop area-wide on-street parking, choose optimum locations for off-street facilities, prepare funding strategies and mechanisms, and select potential management and operational techniques for any future parking facilities. Numerous potential off-street parking facilities have been depicted on Exhibits 24-27.
- (7) Improving traffic safety and circulation. The Brady Village Task Force recommended traffic calming techniques for specific locations along within the area. These concepts are included in this Plan report and were included in the "Recommended List of Physical/Capital Improvement Projects of the Brady Village Infill Neighborhood Task Force" submitted to the City. Additional recommendations have been made for two-way traffic on Cincinnati and Detroit Avenues within Brady Village. The Task Force and the Brady Village Property Owner' Association recognize that the City of Tulsa Traffic Engineering Division will need to

conduct appropriate analysis of this and other traffic recommendations; however, they strongly endorse this objective for their area.

- (8) Provide area lighting for improved safety, security and identity. The Brady Village Task Force has recommended lighting proposals for the area. Lighting issues include four broad lighting categories as follows: (a) roadway lighting to improve vehicular and pedestrian safety and visibility; (b) pedestrian and ornamental lighting for sidewalks and businesses; (c) pinnacle-identity-locator lighting system for helping direct and attract people to the district, enhancing area identity and providing a brand for the area; and (d) building facade and pedestrian lighting to aide vehicle and pedestrian safety and enhance security.
- (9) Streamlining the development permitting process. In 2000, the City of Tulsa established a one-stop review and permitting center at 111 S. Greenwood Avenue in Downtown Tulsa.. This has reduced the time needed and improved efficiency in review of construction and development plans, including those for infill projects.
- (10) Abate or reduce amount of train traffic “whistles and horns”. The blowing of train whistles and horns as trains pass through Downtown Tulsa, although sounding for safety purposes, has a detrimental impact on various businesses and industry according to Brady Village Task Force members and other Downtown representatives. It was determined that a “quiet zone corridor” should be sought for the area. Monies were voted in the 2000 Sales Tax Extension to fund “quiet zone improvements” for several locations in the area. This project is being finalized at this time.
- (11) Supporting crime prevention and security activities. Issues related to specific problem areas were discussed by the Brady Village Task Force. Specific solutions discussed were to increase police presence during festival and major events and to improve the area’s overall lighting, pedestrian visibility and image.

The Plan report that follows contains the vision for Brady Village. It presents the optimum physical improvement envisioned by Brady Village businesses, merchants and property owners and prescribes guidelines for maintaining and enhancing Brady Village’s distinctive urban form. It provides a practical, realistic plan of the Brady Village built environment and practical strategies and actions to achieve Brady Village’s goals.

In summary, the vision is to keep and improve the Brady Village area as Tulsa’s 24/7 Urban Village. This 24/7 Urban Village is to be a mixed use, vital urban environment consisting of the arts, culture and entertainment, communications, teleports and technology, business and industry, and distinctive downtown living. The Brady Village plan seeks to assist and assure that development in Brady Village is of optimum quality and to represent all Brady Village interests equitably, fairly and with vibrant imagination.

BRADY VILLAGE INFILL DEVELOPMENT DESIGN RECOMMENDATIONS

**A COMPONENT OF THE BRADY VILLAGE INFILL NEIGHBORHOOD DETAILED IMPLEMENTATION
PLAN**

PURPOSE

The City of Tulsa identified the Brady Village area as an *Infill Development Study Area* and one of three pilot studies for infill development planning. The purpose of this document is to present infill development guidelines for the anticipated development and redevelopment of the Brady Village neighborhood study area (refer to Exhibit 2).

Brady Village continues to demonstrate substantial infill development and redevelopment potential. The study's recommendations provide assistance and direction for maintaining, improving and enhancing the Brady Village neighborhood. Their adoption and application will serve as an important step incorporating community preferred solutions to community defined issues in Brady Village.

INTRODUCTION

The Tulsa Infill Development Task Force prepared a report for the Mayor and the Tulsa Metropolitan Area Planning Commission in 1999. The report identified several barriers to infill development in Tulsa including the lack of a clear policy for neighborhood redevelopment. In addition, according to the report, the scale, image and context of contemporary commercial structures is often not complementary with older residential neighborhoods. Often, these contemporary commercial structures are not even in context with commercial neighborhoods.

One recommendation of the Tulsa Infill Development Task Force regarding land use was the preparation of infill neighborhood design guidelines. The report states, "The City should consider development and adoption of guidelines regarding appropriate scale and building materials for infill development in the older pedestrian-oriented commercial areas". In addition, the District 1 Plan states the following objective: "A series of urban design guidelines should be established for the downtown to serve as a guide for future development and redevelopment."¹ The Brady Village Infill Neighborhood Task Force, comprised of businesses, merchants and property owners, was formed to address infill development planning in the Brady Village area and to develop design guidelines and standards critical to facilitating high value, compatible infill development. Thus, developing design guidelines and standards for the area was a major component of the Task Force's infill planning efforts.

¹ Land Use District Objective 4.1.2.1.; District 1 Plan, The Tulsa Metropolitan Area Comprehensive Plan; p 1-16.

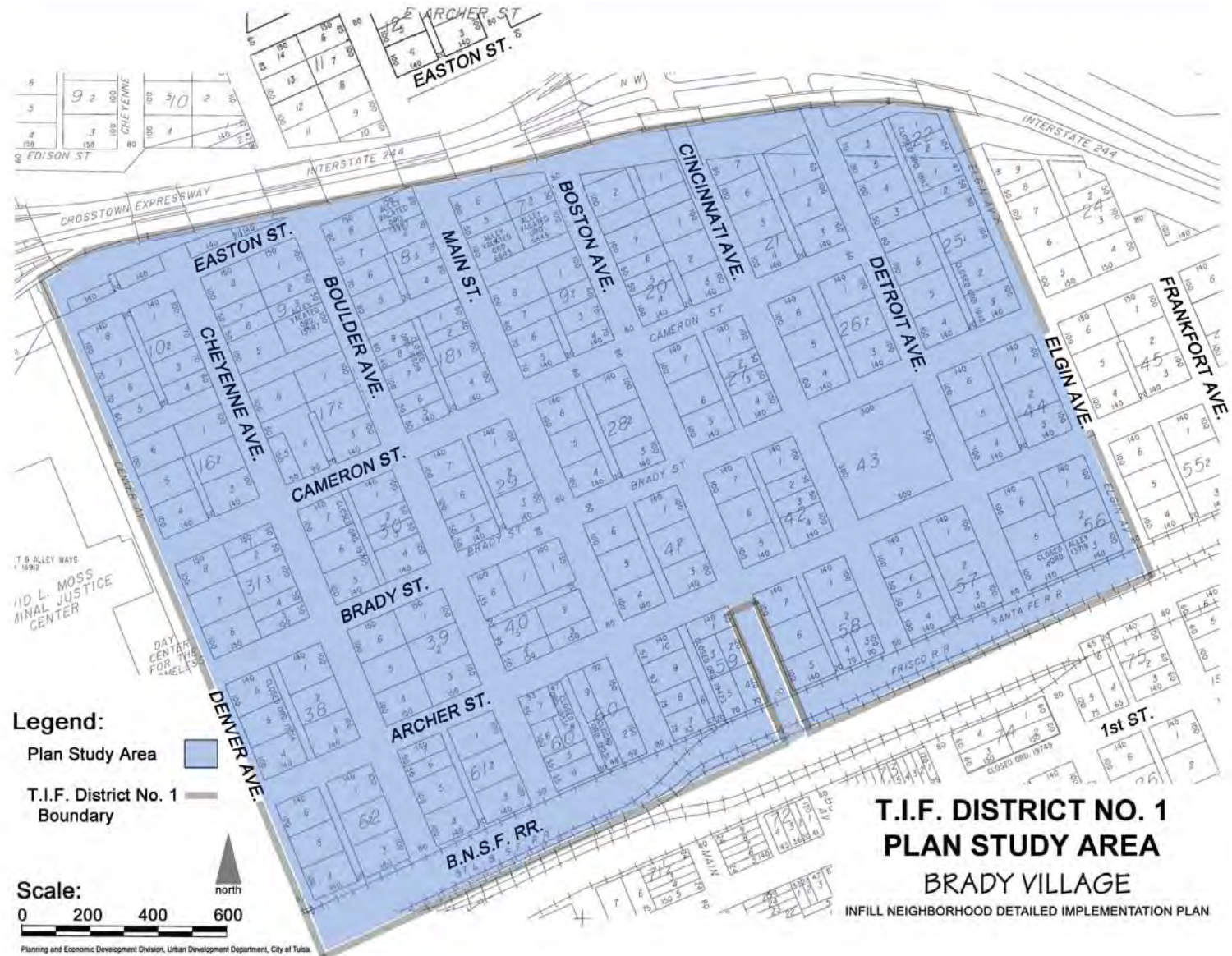
The following design guidelines, policies and standards are suggested as appropriate for urban form in the Brady Village area. These urban form guidelines are based on the District 1 Plan, Tulsa Metropolitan Area Comprehensive Plan policies for the Brady Village area, recommendations of the Tulsa Infill Study Task Force, recommendations of the Brady Village Infill Task Force, and special studies within the study area. It is the belief of the Brady Village Infill Task Force that the application of appropriate design guidelines can help insure economic success, compatibility and acceptance of area redevelopment projects. Application of the guidelines will help (a) balance business, industrial, commercial, office, public and residential demands within the Brady Village area, (b) help improve and enhance the connectivity of Brady Village with all of Downtown, the O.S.U.-Tulsa campus, Brady Heights, Greenwood Cultural Center, the Oklahoma Jazz Hall of Fame, East Village and other critical environs, (c) facilitate appropriate pedestrian scale, improve area traffic safety and movement patterns, and (c) encourage high-quality, high-value public and private development and improvements.

OVERVIEW

The Brady Village Study Area is depicted generally in Exhibit 2 and in greater detail in Exhibit 3. The Brady Village Study Area corresponds to the boundaries of the Brady Village T.I.F. District No. 1. The Tulsa Comprehensive Plan recognizes Brady Village as a component of the District 1 Plan. Brady Village contains portions of several “Identity Areas” depicted in the District 1 Plan and include the Coliseum Area, University Commons, Greenwood District, Art/Entertainment District, Expanded Core, and Retail Center. These Identity Areas were identified and adopted in the December 21, 1989 Major Update to the District 1 Plan and were endorsed in subsequent amendments to the District 1 Plan through March 3, 1995. These Identity Areas are depicted in “The District 1 Plan Map” shown in Appendix C of this report. Appendix D shows Brady Village T.I.F. District No. 1 boundaries with respect to these Identity Areas. Refer to Appendix E for recent pictures of the Brady Village area.

The recommended Brady Village guidelines, policies and standards are organized according to general land use, intensity, transportation, overall design, and street furniture, features and improvements. Those guidelines, policies and standards recommended for the Brady Village Infill Neighborhood are set forth below.

EXHIBIT 2
BRADY
VILLAGE
T.I.F. NO. 1
AND STUDY
AREA





THE BRADY VILLAGE AREA

1. **Overall Design Policies.** The Brady Village area is an urban village with its own special identity, sense of community, pattern of development and unique characteristics. The protection, preservation and enhancement of this higher density urban village is of paramount importance. This urban village is intended to be an area of business, entertainment and residential activity associated with a vibrant, colorful atmosphere and character 24 hours a day, 7 days a week (“24/7”). There are certain design policies and standards appropriate for improvements in the study area and they include the following:

A. General.

- (1) Protect the pedestrian and enhance the pedestrian environment and scale;
- (2) Give clear indications of pedestrian activity zones in heavy pedestrian areas;
- (3) Improve the visibility and protection of pedestrians in heavy vehicle traffic areas;
- (4) Slow vehicular traffic in heavy pedestrian traffic areas;
- (5) Reinforce safe and efficient movement of vehicular traffic; and
- (6) Preserve and reuse existing buildings.

B. Land Use.

- (1) Encourage high quality residential, office, entertainment, commercial and industrial infill development. No one single land use dominates or should dominate to the exclusion of other vital businesses, interests and activities. Brady Village is intended as a higher intensity mixed use urban village;
- (2) Brady Village is intended to continue and increase its emphasis, suitability and special accommodation of pedestrian traffic. Particular attention is to be provided in the design of new and replacement sidewalks, crosswalks, use of storefront displays and merchandising, and provision of sidewalk dining areas.

- (3) Recommend policies and strategies to maintain and improve high quality development and property values which are to be accomplished through techniques that provide positive incentives for compliance and development. As a strategy to facilitate and promote development consistent with Brady Village goals, policies, standards and guidelines, and to promote and encourage quality infill development the following policies are suggested:
 - (a) Support all CBD Central Business District rezoning requests within Brady Village.
 - (b) Amend the District 1 Plan of the Tulsa Metropolitan Area Comprehensive Plan to adopt the enclosed development design guidelines for the Brady Village area.
 - (c) Amend the Tulsa Zoning Code to require the continuation of an unwritten policy that obligates a meeting be held by developers with neighborhood representatives prior to formal application for a PUD in order to provide better understanding of the needs and desires of developers and existing property owners.
 - (d) Reemphasize good lines of communication and coordination necessary to insure those development standards adopted for Brady Village will be automatically and systematically applied to building permit applications in the review of building and construction plans.
 - (e) Provide development and redevelopment incentives within the Brady Village area. Such incentives might include but are not limited to:
 - (i) Reducing substantially or eliminating building permit fees for a specified period of time within the designated Brady Village portion of District 1;
 - (ii) Continue to assist development in Brady Village and all of the District 1 through continuation of the Fire Suppression Assistance Program of the City of Tulsa;

C. Transportation

- (1) Continue the use of two-way traffic patterns in existing two-way traffic areas and provide two-way traffic movement in all appropriate areas presently having one-way vehicle traffic movement (refer to Appendix L for traffic circulation study for portion of Brady Village).;

- (2) Maintain and improve the connection of Brady Village area streets to all of downtown Tulsa and adjoining areas;
- (3) Maintain and improve bicycle routes through Brady Village and provide an additional route to the Archer Street route (refer to Appendix F).
- (4) Maintain and improve the connection of Brady Village area bridges to all of downtown Tulsa including the reopening of Boulder Avenue Bridge to pedestrian and vehicular traffic;
- (5) Provide grade separated crossings or enhanced railroad crossing protection for vehicles and pedestrians to improve traffic safety and assist in establishment of a “quiet corridor zone” for train traffic within Brady Village and downtown Tulsa;

D. Parking.

- (1) Customer and tenant parking has been historically provided on-street, in the rear of buildings, in off-street parking lots or adjacent parking lots. This pattern is to continue with the addition of off-street parking structures placed behind primary land uses. Major parking structures are intended to be mixed use with non-parking uses fronting at street level to adjoining streets.
- (2) Provide angle parking on both sides of the following streets in Brady Village: Archer Street, Brady Street, Cameron Street, Cheyenne Avenue, Boulder Avenue, Main Street, Boston Avenue and Elgin Avenue;
- (3) Provide angle-parking on one side and parallel parking on one side of Cincinnati Avenue and Detroit Avenue;
- (4) Provide parallel parking on one side only of Easton Street;
- (5) Provide additional off-street parking facilities through private, public or joint private-public partnerships. Parking lots and multiple-level parking facilities should be placed at the back of buildings, or developed as mixed use buildings with commercial, office, and/or entertainment uses at street-level fronting on adjoining streets. Refer to Appendix M for examples of appropriate infill mixed use parking facilities; and

E. Urban Design.

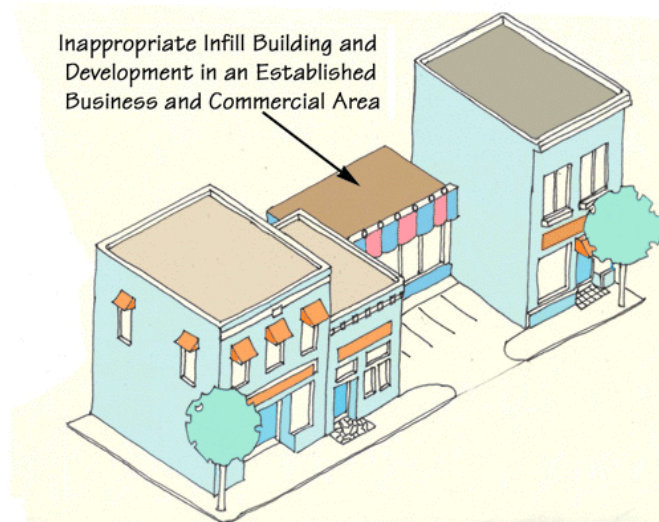
- (1) Respect and preserve the existing building stock located in Brady Village. Particular attention should be given to preserving masonry, brick and stone structures in Brady Village. These buildings represent some of the oldest structures in Tulsa and provide a key link to Tulsa's past and represent a portion of Tulsa's special character that cannot be replaced. These buildings are a critical resource in preserving Tulsa's identity and history, and provide excellent resources for establishing Brady Village and Tulsa's unique "brand" nationally.
- (2) Businesses should remain, develop or redevelop with buildings constructed near or along the front property line(s). Zero setback from the front property line(s) is encouraged. Variations in zero setback from the property line(s) may be appropriate where the resulting setback provides greater accommodation for pedestrian movement, sidewalk dining areas, enhanced sidewalk business entries, does not dramatically alter established sight-lines, provides interest, and emphasizes the pedestrian environment.
- (3) The pedestrian environment is intended to be inviting and in keeping with the historical and mixed use character of Brady Village's "village atmosphere." Sidewalks should provide for good, safe pedestrian movement. Outdoor dining areas are encouraged. The provision of overhead cover for pedestrians is encouraged. Protection may be in the form of fabric awnings, projections from the buildings arcades or any combination of the above.
- (4) On-street curbside parking is to continue in the area. Curb-extensions are to be provided in appropriate locations to eliminate parking conflicts near intersections and define and add on-street parking spaces. Exhibit 13 depicts typical curb-extensions or "bump-outs." Refer to Appendix J for examples of "bump-outs." These improvements are to be consistent with the standards for Overall Design and Street Furniture, Features and Improvements described above. Typical improvements are depicted in the Concept Development Plan Venues for Brady Village - Exhibits 24 through 27.
- (5) Avoid and reduce conflicts between utility poles, lines and equipment and pedestrians and vehicles.

- (6) Respect adjacent buildings through consideration of mass, rhythm, scale, setback, height, building materials, texture and related design elements when developing or redeveloping in the area. Refer to Exhibit 4.

E

EXHIBIT 4

BRADY VILLAGE INFILL DEVELOPMENT



INAPPROPRIATE INFILL DEVELOPMENT

SCALE - Avoid buildings that in height, width or massing violate the scale of the area.

HEIGHT - Avoid new construction that varies greatly in height and number of stories.

SETBACK - Avoid violating the existing setback patterns in the area.

MASSING - Avoid monolithic forms or forms which dramatically dominate space.

RHYTHM - Avoid disrupting or violating general visual patterns.

APPROPRIATE INFILL DEVELOPMENT

SCALE - Relate size and proportions of new infill construction to adjacent buildings.

HEIGHT - Relate overall height and typical number of stories.

SETBACK - Conform to existing patterns of setback.

RHYTHM - Some variety of wall areas with doors, windows, cornices and architectural features is appropriate when in character with area buildings.

MASSING - Conform general to size and amount of space consumed by adjacent buildings.

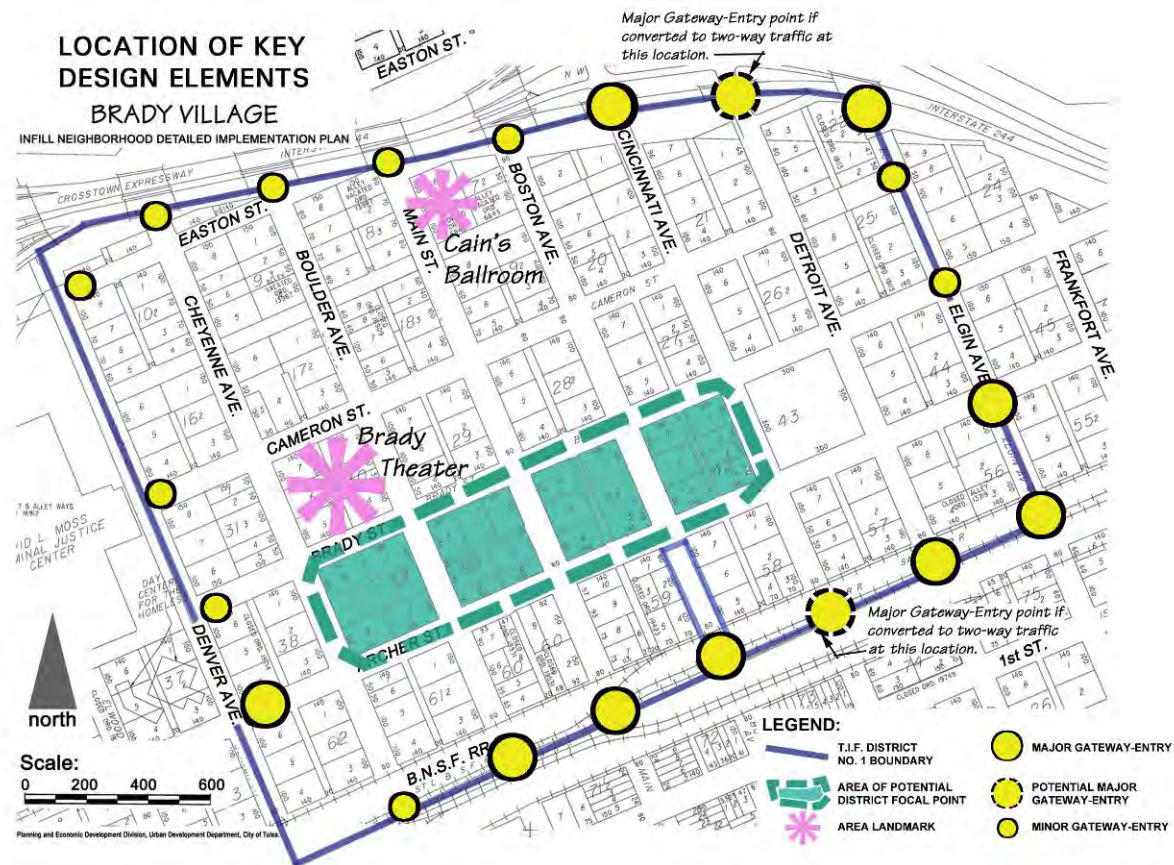


- (7) Encourage the construction of replacement or new buildings and structures along the front property lines of property in Brady Village;
- (8) Establish a strong sense of entry into and out from the Brady Village area. Techniques that are recommended include development of an identity-locator-pinnacle lighting system; provision of intersection improvements and special pedestrian crossing treatments; provision of special lighting, landscaping and/or murals at the Interstate 244 bridge underpasses; improved maintenance painting at the Interstate 244 bridge underpasses; and special murals, banners and signs at appropriate entry points into Brady Village.
- (9) Establish a strong sense of identity, branding of the area, and improved safety and visibility of Brady Village through provision of the following:
 - (a) an “identity-locator-pinnacle” lighting system;
 - (b) additional on-street angled parking;
 - (c) enhanced pedestrian elements including replacement or construction of new sidewalks and curbs;
 - (d) curb-extensions or “bump-outs” at pedestrian crosswalks;
 - (e) crosswalks and intersection improvements at street intersections;
 - (f) replacement or new highly durable, low maintenance, thin canopy street trees;
 - (g) replacement or new facade lighting of area buildings;
 - (h) replacement or new higher visibility, pedestrian friendly roadway lighting;
 - (i) replacement or new pedestrian street lights;
 - (j) plaza(s) (refer to Appendix G); and
 - (k) park(s) (refer to Appendix G).

- (6) Support special events and activities within Brady Village and provide additional public safety and security measures during such special events and activities.
- (7) Provide a continuity of public design for streetscape and specific street improvements in Brady Village. Provide a common design theme, continuity and identity throughout the business area through the use of street furniture, street trees, sidewalks, and other public improvement features.
- (8) Determine and /or establish an ongoing program and/or authority for funding, placing, managing, operating and maintaining specific public and/or private-public improvements Brady Village. Programs(s) are recommended for: (a) streetscape and landscape improvements (e.g., trash receptacles, benches, sidewalks, artwork, entries and pedestrian lamp posts); (b) off-street parking facilities, and (c) “identity-locator-pinnacle” light system.
- (9) Refuse containers for business operations will be placed from public view or screened with appropriate materials and gated to allow closure from public view and to control the spreading of trash. Appropriate screening includes wooden fences with cap-rails and base-rails, landscaping, or masonry walls or any combination of the above, so long as the refuse containers are appropriately screened from view and provide ready access for the businesses.
- (10) Seek to provide incentives to develop and design new infill and redevelopment projects in a manner that is compatible with area development and consistent with the Brady Village development design guidelines. Incentives which have been used in other communities to foster appropriate infill development and which might be appropriate in Brady Village are listed in Appendix H. Such incentives should be studied in greater detail by the appropriate public or private entities to determine which should be used in Tulsa.

- F. Location of Key Design Elements.** Key design elements in the study area are depicted in several exhibits. Broader or more general design elements are depicted in Exhibit 5 and include the boundary of Brady Village, major and minor gateway-entries into Brady Village, and area of potential district focus, and two area landmarks. These area icons are the Brady Theater (Ole Lady o' Brady) and the Cain's Ballroom. These landmarks provide a continuing, positive identity for Brady Village that should be preserved and enhanced. These two buildings also offer an excellent basis on which to help build, enhance and link the area's unique historical character, brand and image to its emerging future.

EXHIBIT 5

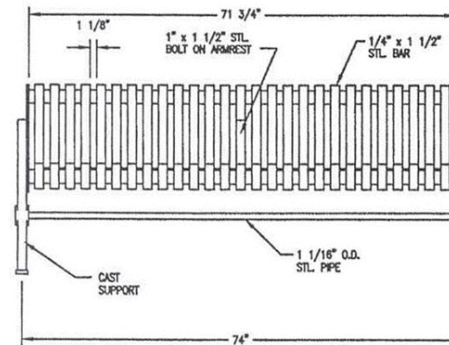
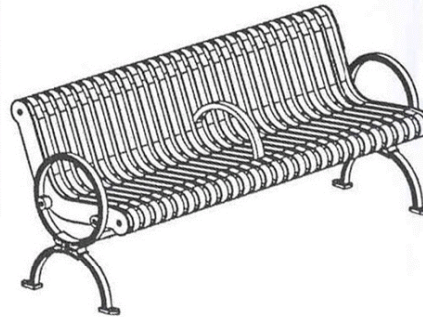


2. **Specific Design Policies.** Specific policies, guidelines and standards have been recommended for the study area and planned major improvements. These specific design policies are guided by the general key design elements. It is intended that streetscape and specific street improvements have a visual consistency throughout the Brady Village area that will provide continuity, theme, linkage and identification of the Brady Village area. The major improvements include: street furniture; paving materials; street trees and plant materials; lighting; and public art and monuments. The policies, guidelines and standards for these improvements are set forth below.
 - A. **Street Furniture.** Street furniture should be strong and highly durable, requiring low maintenance and the need for keeping a large inventory of spare parts. It is intended that high quality fixtures are provided which provide cost savings due to longer life expectancy. A standard family of colors should be identified for all public Brady Village street furniture, and any metal parts should be painted with adopted colors. Dark, warm and glossy colors of black, green or blue are recommended. It is desirable that any private street furniture compliment public street furniture. Street furniture includes benches, trash receptacles, bicycle racks, and bollards. Examples of street furniture are depicted in Appendix I. Street furniture standards are as follows:

- (a) **Benches.** Public benches are an important place to rest, gather, wait and watch the activity of the marketplace. Placement of benches should not interfere with pedestrian movement and be based on the need for benches. Metal, highly durable benches with backs are recommended. Benches should be placed in well shaded locations to reduce potential discomfort of direct sun. Benches provided by the private sector which are to be placed in public space should be consistent with these standards. Refer to Exhibit 6 for “typical decorative bench.”

Typical Decorative Bench

BRADY VILLAGE
INFILL NEIGHBORHOOD DETAILED IMPLEMENTATION PLAN

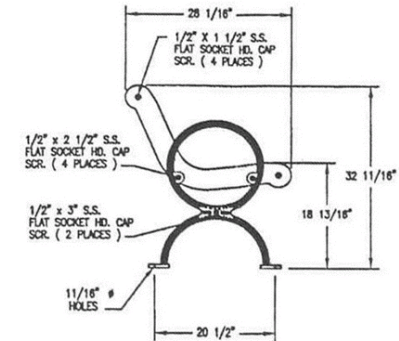


Typical Decorative Bench:

- Equal to Dumor Model #119-60-3AR, 6' w/Center Armrest, Surface mount.
- Dumor, Inc.
- P.O. Box 142
- Mifflintown, PA 17059-0142

NOTES

- 1.) ALL STL. MEMBERS COATED W/ ZINC RICH EPOXY THEN POLYESTER POWDER COATED (SEE COLOR OPTIONS)
- 2.) 1/2" X 3 3/4" PLTD. EXPANSION ANCHOR BOLTS PROVIDED.

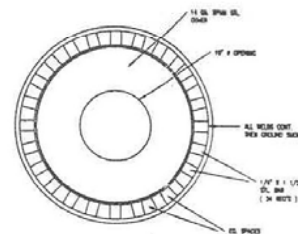
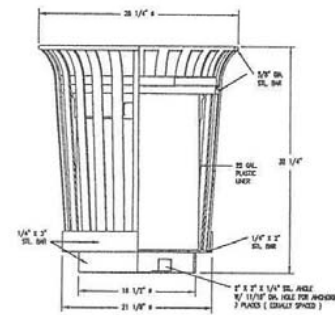


SOURCE: "Downtown Steel Furniture Standards - Site Amenities", Downtown Tulsa Unlimited.
Planning and Economic Development Division, Urban Development Department, City of Tulsa.

EXHIBIT 6

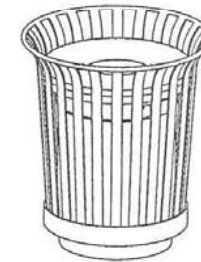
- (b) **Trash Receptacles.** Public trash receptacles can provide an important component of the systematic control of litter and trash clutter. Trash receptacles provide an important role in the care and maintenance of a positive image of the street and environment. Refer to Exhibit 7 for a “typical trash receptacle.”

EXHIBIT 7



Typical Trash Receptacle

BRADY VILLAGE
INFILL NEIGHBORHOOD DETAILED IMPLEMENTATION PLAN



Typical Decorative Trash Receptacle:

- Equal to Dumor Model#84-22, 22 gal.,
Surface mount.
Dumor, Inc.
P.O. Box 142
Mifflintown, PA 17059-0142

NOTES

- 1.) ALL STL. MEMBERS COATED W/ ZINC RICH EPOXY THEN POLYESTER POWDER COATED (SEE COLOR OPTIONS)
- 2.) 1/2" X 3 3/4" PLTD. EXPANSION ANCHOR BOLTS PROVIDED.

SOURCE: "Downtown Steel Furniture Standards - Site Amenities", Downtown Tulsa Unlimited, Planning and Economic Development Division, Urban Development Department, City of Tulsa


- (c) **Bicycle Racks.**
The emphasis on the pedestrian environment will include additional use of bicycles in the Brady Village area. Bicycle racks are to provide a visually appealing means of securing bikes, reduce the use of trees, benches or trash receptacles for securing bikes, decrease incidents of damage, and accommodate bicycle enthusiasts and recreational riders in the area. Bicycle racks should be metal, highly durable and painted in the street furniture family of colors.

**EXHIBIT 8 -
TYPICAL
BICYCLE RACK
(OPTION 1)**

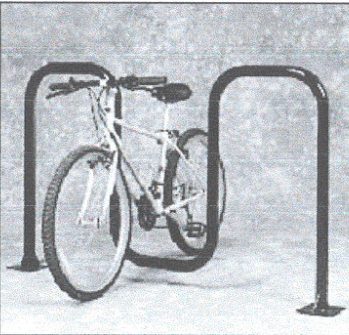
Typical Bicycle Racks (Option 1)

BRADY VILLAGE
 INFILL NEIGHBORHOOD DETAILED IMPLEMENTATION PLAN

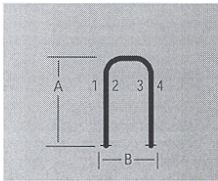
1000



2000



1042 4 bike



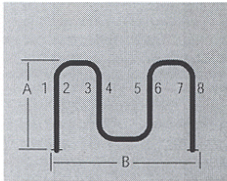
**Model 1042
with Base Plate**

A _____ 39" 99.06 cm

B _____ 26" 66.04 cm

Weight ____ 18 lbs 8.16 kg

2082 8 bike



**Model 2082
with Base Plate**

A _____ 39" 99.06 cm

B _____ 66" 167.44 cm

Weight ____ 31 lbs 14.06 kg

Typical Bicycle Racks:

- Equal to American Bicycle Security Company "Viper Rack Series" Model # 1042 and #2082, surface mount.

American Bicycle Security Company
 P.O. Bo 7359
 Ventura, CA 93006
 Phone: 1-800-245-3723 or 805-933-3688
 E-mail: turtle@ameribike.com
 URL: <http://www.ameribike.com/>

NOTES:

- 1) All MIG welded construction. Tubing is 2"OD w/ 0.09" thickness; finished racks are grit-blasted, polyester powder coated, oven cured.
- 2) Base plates shall be 6" square w/ gussets; 8 expansion anchors per base plate equal to PN#MK9000; concrete embedment is recommended.

SOURCE: Planning and Economic Development Division, Urban Development Department, City of Tulsa.

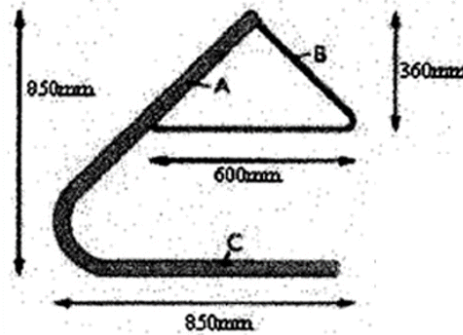
Bicycle racks should be placed in high use areas and as not to impede pedestrian movement. Refer to Exhibits 8 and 9 for “typical bicycle racks.”

Typical Bicycle Racks (Option 2)

BRADY VILLAGE
INFILL NEIGHBORHOOD DETAILED IMPLEMENTATION PLAN



(Stainless steel finish shown; standard color "Comet Black" specified)





Typical Bicycle Racks:

- Equal to Cora EXPO 'W' series bicycle rack Model #W2704 (3-5 bikes capacity) and Model #W4506 (5-7 bikes capacity); surface mount; heavy duty high quality steel construction.
- Cora Bike Rack Pty Ltd
Phone: 1-800-354-8624

NOTES:

- 1) Heavy duty high quality steel construction w/ 12mm polyester plastic coating color finish; standard color "Comet Black".
- 2) 2 x 12mm x 125 masonry, stainless steel M12 Dynabolts w/ tamper-proof fasteners provided.

Model	EXPO 2704	EXPO 4506
		
	W2704	W4506
Bike Capacity	3-5 Bikes	5-7 Bikes
Length	800mm	1250mm

A. 50 NB 3.6mm MD Pipe

B. 19 mm Round Bar

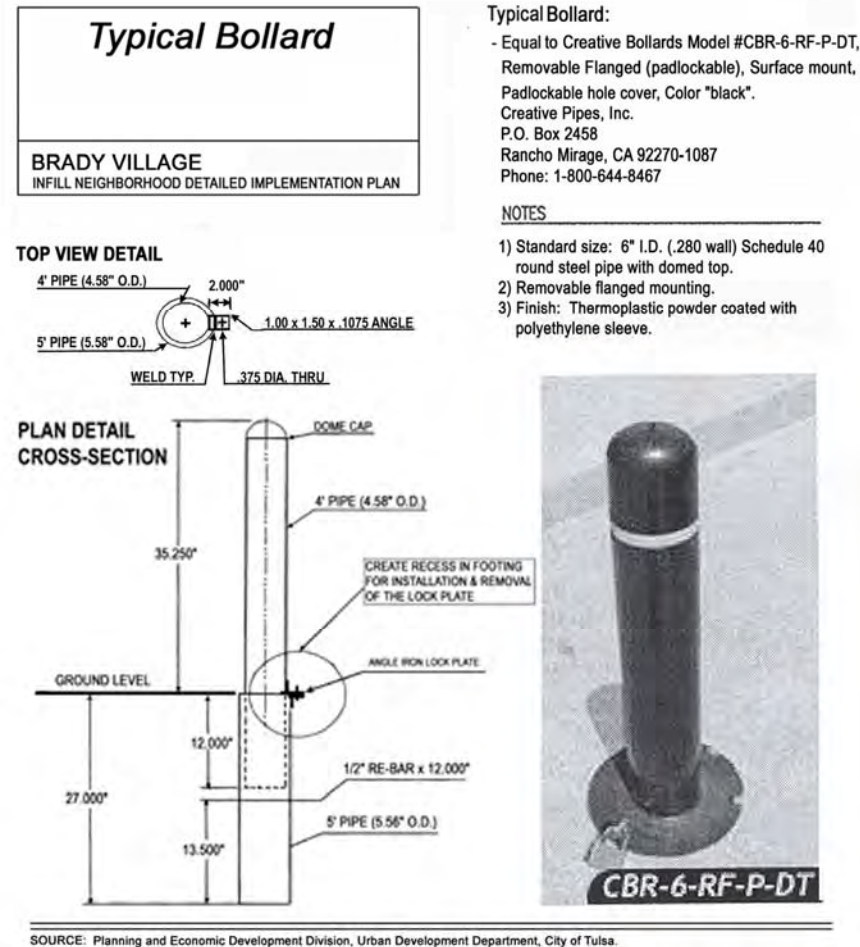
C. 12 mm hole to fit M12 Dynabolt

EXHIBIT 9 - TYPICAL BICYCLE RACK (OPTION 2)

SOURCE: Planning and Economic Development Division, Urban Development Department, City of Tulsa.

- (d) **Bollards.** Bollards can be used as access features to restrict vehicles from pedestrian priority or pedestrian only areas. They may be used to provide added separation and safety from vehicles or outdoor dining spaces near the curb line. Bollards may also be used to define and restrict parking areas and spaces for motorcycles and scooters. Removable bollards should be used where service or emergency access is required. “Tilt-down” bollards may be appropriate in areas of heavy pedestrian or bicycle traffic. Where provided to add separation and protection from vehicles, bollards should be metal, highly durable and painted in the street furniture family of colors. Where provided to add visual clues and restrict vehicle traffic, bollards should be removable or flexible and made of a heavy duty, low maintenance material painted in the street furniture family of colors. Refer to Exhibit 10 for a “typical bollard.”

EXHIBIT 10 - TYPICAL



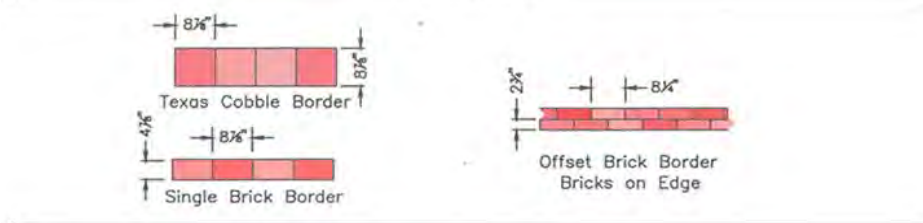
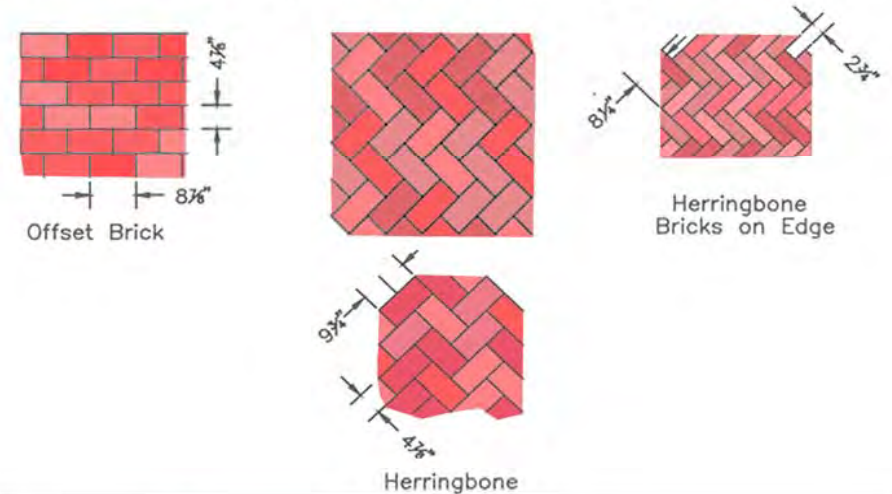
- B. **Paving Materials.** Roadway and sidewalk surfaces are the largest, most visible and most expensive element of the Brady Village streetscape. It is intended that the street and sidewalk surfaces contribute a positive, attractive image to the Brady Village environment. Paving surfaces afford one of the greatest opportunities for establishing and continuing a unified identity for Brady Village. Paving surfaces are particularly important at street intersections, pedestrian crosswalks, “bump-outs”, sidewalks and business entrances.
- (1) **Street Paving and Parking Lots.** Brushed or scoured concrete surfaces, brick pavers, textured concrete or asphalt, or other suitable material and techniques are encouraged to emphasize street intersections. The use of textured surfaces provides a unifying theme, adds identity, slows traffic and enhances the pedestrian environment. The use of textured paving is appropriate at particular intersections and locations in Brady Village, and is most beneficial when designed in conjunction with pedestrian crosswalks. Similar paving surface treatment is also appropriate for off-street parking areas, particularly in areas where pedestrian movement needs emphasis. The materials will at a minimum be of different texture and color than the standard street surfacing. The common design patterns for the street, street intersection or parking lot receiving special treatment are depicted in Exhibit 11.
 - (2) **Pedestrian Crosswalks.** Pedestrian crosswalks are to be provided and designed with common elements and materials for all major arterial and collector street intersections. These crossings are intended to maintain and improve pedestrian safety, beautify the Brady Village area, and enhance the business and residential identity of the area. Design elements of these pedestrian crossings will include brick pavers, textured concrete or asphalt, or other suitable material which will emphasize where pedestrians cross the intersecting streets. The materials will at a minimum be of different texture and color than the street surfacing. These crossings are intended to maintain and improve pedestrian safety, beautify the Brady Village area, and enhance the business and residential identity of the area. Painted crosswalks are to be provided in those locations where textured paving cannot be provided. The common design patterns for the typical pedestrian crosswalk receiving special treatment are depicted in Exhibit 11. Crosswalk improvements are to be designed with common elements and materials and consistent with the standards for Overall Design and Specific Design described herein. A Concept Illustration of a typical design for these crosswalks and intersection improvements is depicted in Exhibit 13 which also depicts typical “bump-outs”. Refer to Appendix J for examples of “bump-outs”. The Concept Development Plan Venues for Brady Village depict the location of these improvements.

- (3) **Sidewalks.** A system of sidewalks is intended for all arterial and collector streets in Brady Village. Crosswalks with textured paving are also to be provided on area streets with similar or complimentary patterns. Concrete sidewalks are the most common paving surfaces provided. Concrete sidewalks should be constructed in lengths which can be easily replaced whenever access to utilities is required. Special sidewalk treatment which includes the use of textured paving surfaces and/or colored materials is encouraged as a design element throughout Brady Village. The provision of a quality sidewalk system is a critical component of preserving and improving the pedestrian environment. It provides another design tool to link and unify the Brady Village area. The common design patterns for sidewalks receiving special treatment are depicted in 11 and some examples are shown in Appendix J.

EXHIBIT 11

TYPICAL TEXTURED PAVEMENT PATTERNS FOR SIDEWALKS, PARKING LOTS AND STREETS

(Use in sidewalks and pedestrian street crossings at select locations)

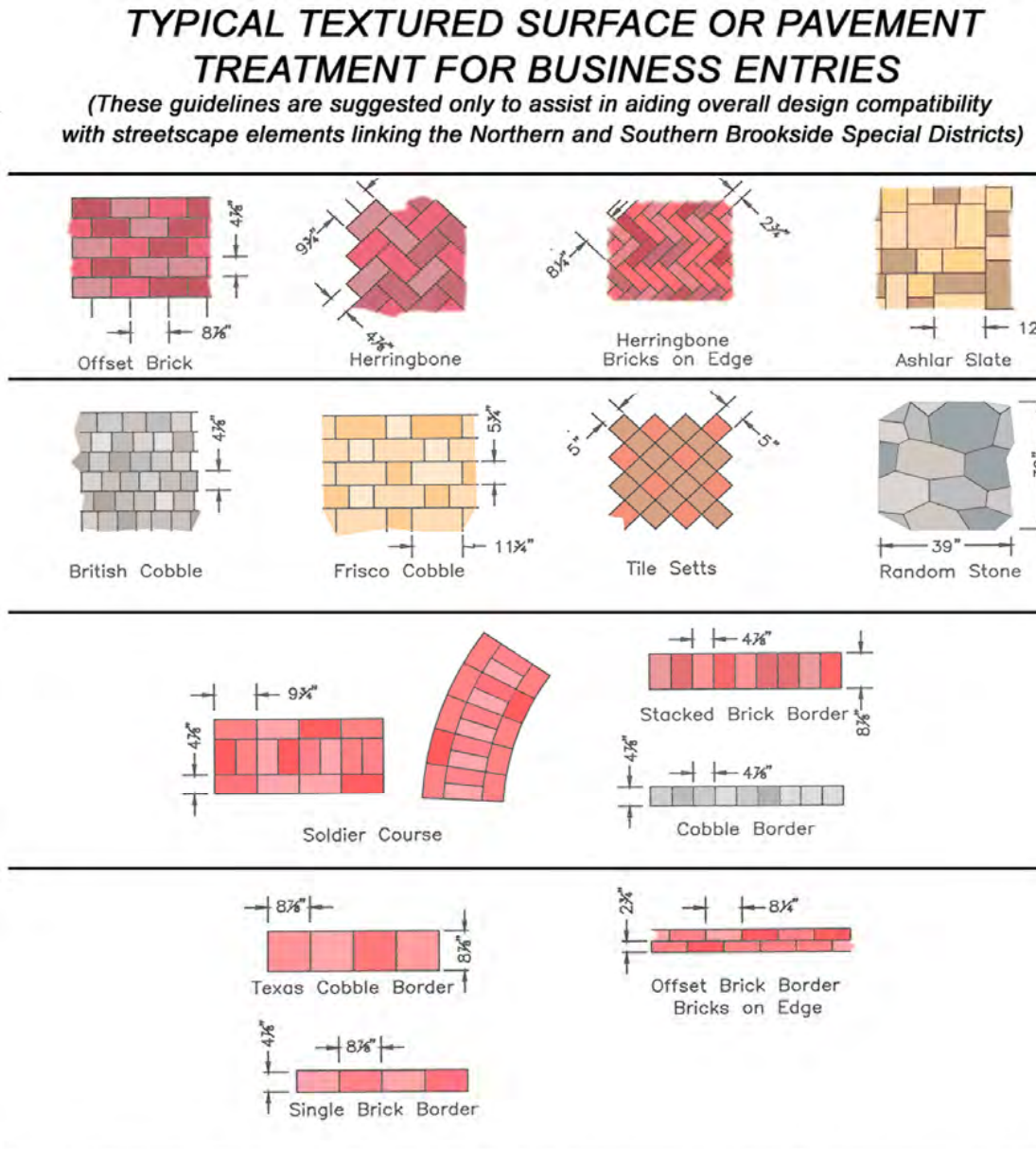


Decorative Arch

(Use in plazas, parking lot lane or selected street intersections)

SOURCE: "Street Print Pavement Texturing", Integrated Paving Concepts Inc., October, 2000, and Planning and Economic Development Division, Urban Development Department, City of Tulsa

- (4) **Sidewalk Business Entries.** Sidewalk entries to businesses are suitable for special treatment and should emphasize pedestrian safety, provide a sense of entry and add to the variety of visual imagery in Brady Village. Entry areas constructed by and in front of businesses within the sidewalks should be compatible with existing development and development design guidelines for sidewalks. The common design patterns for the typical textured sidewalk entry are depicted in Exhibit 12.

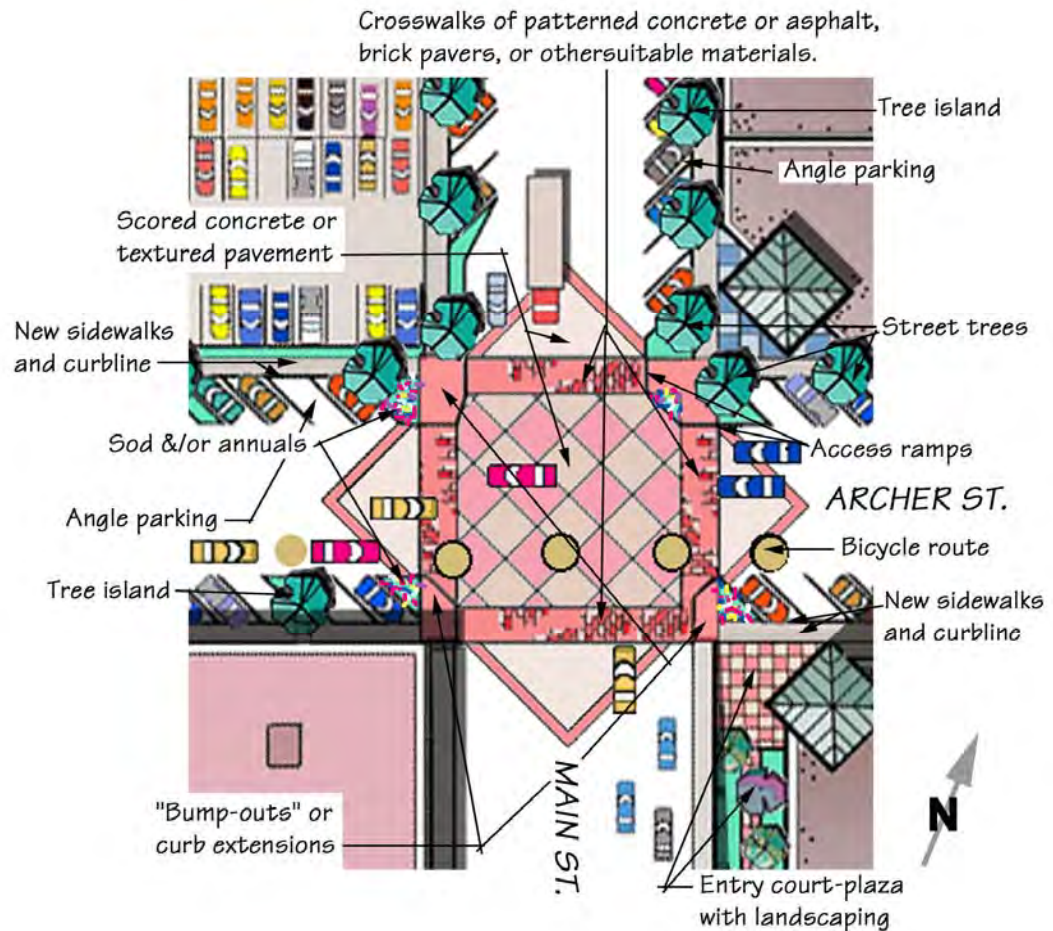


SOURCE: "Street Print Pavement Texturing", Integrated Paving Concepts Inc., October, 2000, and Planning and Economic Development Division, Urban Development Department, City of Tulsa.

EXHIBIT 12

- (5) **“Bump-Outs” or Curb Extensions.** Curb modifications are to be provided at specific locations within the Brady Village area. These modifications are intended to improve pedestrian safety by narrowing the distance of street crossings, reducing vehicle speeds and accentuate the pedestrian environment. They also establish area identity, add space for street plants and furnishings, eliminate parking conflicts near intersections, and define and add on-street parking spaces. These features are intended to have a design theme and use materials that blend well with enhanced pedestrian crosswalks. The common design elements and features for the typical “bump-out” or curb extension are depicted in Exhibits 13. Refer to Appendix J for examples.

EXHIBIT 13 -
TYPICAL “BUMP-OUTS” OR CURB
EXTENSIONS
and
TYPICAL CONCEPT
DESIGN FOR
CROSSWALKS AND
STREET
INTERSECTION
IMPROVEMENTS



- C. **Street Trees and Plant Materials.** Urban form is greatly influenced by the inclusion of plants, particularly trees, in design of public and private space. Plants play an important role in establishing human scale and maintaining an attractive pedestrian environment. Plant materials of particular value to street design are shade trees, evergreen trees, flowering and ornamental trees, shrubs, hedges and ground cover. Trees serve several important functions. They provide shade in warmer months, seasonal color in the fall, some protection from the elements in colder months, visual interest and an additional unifying design element for Brady Village. It is intended that certain specimens of trees will be used fairly regularly to provide a visual link throughout the area. However, the selective use of other species is encouraged to add interest.

Plant and tree species which are particularly suitable for Tulsa's environment have been identified for the City of Tulsa. Those which are highly adaptable, easily maintained and well-suited to the urban environment should be selected. Site characteristics and design goals will greatly determine which plant materials are most suitable at specific locations. It is important that trees planted in Business Areas be those which have an open canopy which will not obstruct business signs. The spacing and placement of trees will be such that business signs are not adversely affected. It is very important that plants do not obstruct pedestrian access. The City of Tulsa urban forester and landscape architects are good contacts for assistance in plant selection.

Included with these standards for street trees are the design guidelines for in-ground tree planters (tree pits) and tree grates. These items are depicted in Exhibits 14 and 15.

(1) **In-Ground Tree Planters (Tree Pits).** New street

(Tree Pits). New street trees should be planted with in-ground planters. A planting bed promotes a healthier tree and allows for better water and nutrient absorption and encourages a better street environment. The risk of soil compaction is reduced and additional room is provided for root growth. Pedestrian traffic on the planting areas can be discouraged by elevating the planters with a coping edge or curb edge slightly higher than the adjoining sidewalk. A minimum of eight feet (8') feet of walking space between adjoining buildings and in-ground planters should be maintained where possible, and ten feet (10") of walking space is recommended. A typical in-ground tree planter (tree pit) and tree grate installation is shown in Exhibit 14.

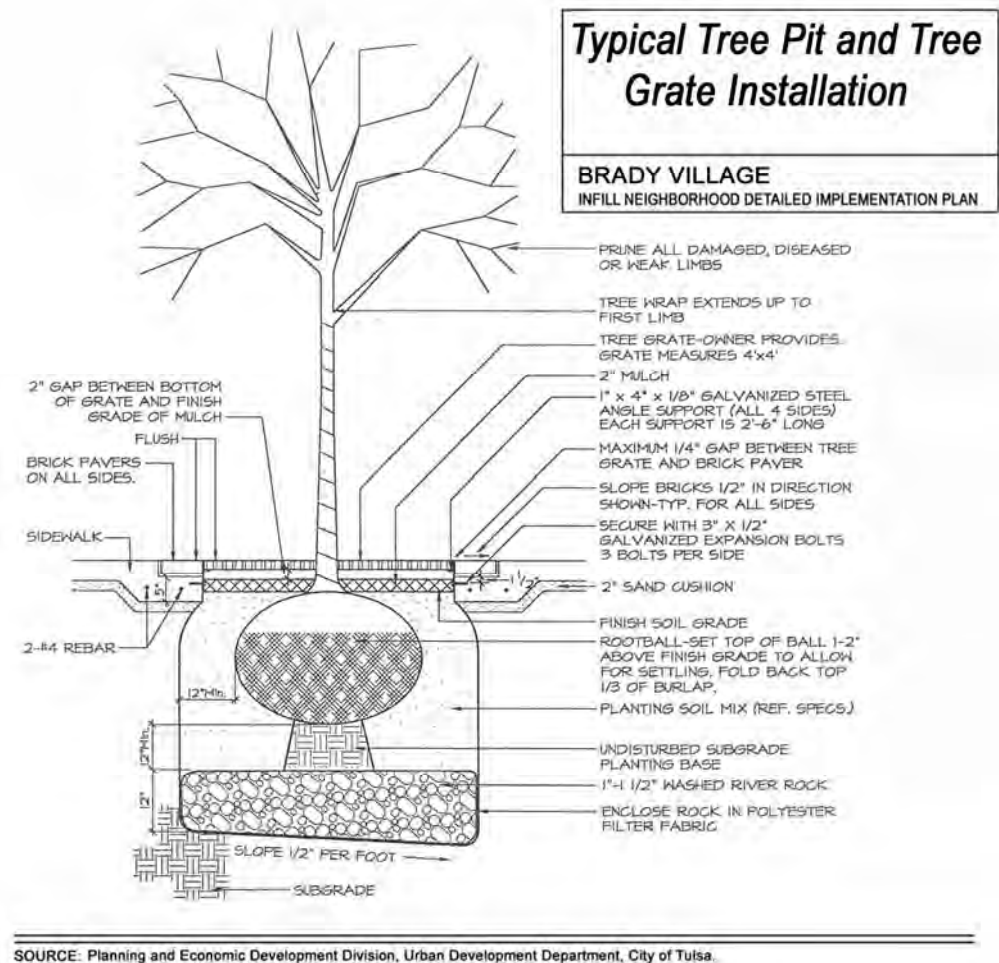


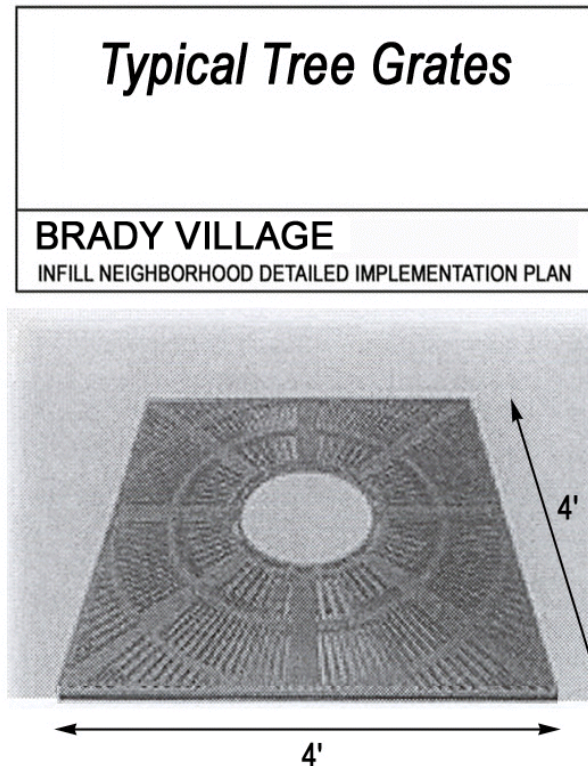
EXHIBIT 14

- (2) **Tree Grates.** Tree grates are recommended only where trees are to be provided where sidewalk widths are less than eight (8') and dimensions are too narrow to allow in-ground tree planters. Tree grates should be provided only in areas which have been studied in detail and where pedestrian traffic would occur over tree pits. Areas in which tree grates are required should be linear with a minimum dimension of four-feet-by-eight (4' x 8') or six-feet-by-six-feet (6' x 6').

Tree grates are to be cast iron and have small openings and minimize tripping hazards, be of a standard design and dimensions, and have consistent, specialized installation techniques defined. Tree pits greater than eight-feet-by-eight-feet (8' x 8'), without tree grates, are preferred.

New sidewalks should be provided so that the sidewalk is suspended over a continuous planting trench with amended soil. As an alternate to this technique, sand-set-paving-units can be used in a continuous band between the street trees to allow for drainage and root growth.

Root aeration strips are also recommended between and parallel the curb-line between tree pits. These techniques or their equivalents are intended to provide a healthy environment for trees and increased life spans, root growth, lower tree maintenance costs and reduced sidewalk damage. Refer to Exhibit 15 for "typical tree grates."



Typical Tree Grates:

- Equal to Urban Accessories Cast Iron per ASTM A48 class 35b or better; Model "COHO"; 4' square.

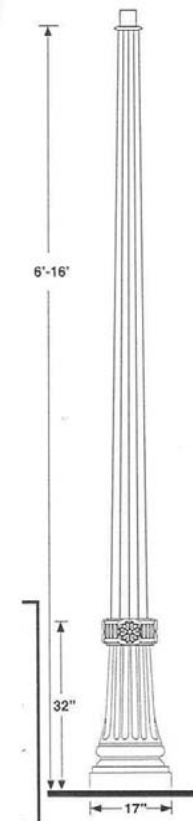
NOTES:

- 1) Polyester powder coated; standard color black.
- 2) Castings 1/8" undersized/frames 5/8" oversized.

- D. **Lighting.** It is intended that adequate roadway “white light” be provided within the Brady Village area that compliments its unique and pedestrian character. In particular, a pedestrian lighting system is intended. It is to serve as a strong design element. Lighting is intended to provide added security and safety and improve use and access to parking areas within the District. Lighting will be designed to be considerate of adjoining uses. Lighting fixtures may vary from Brady Village to other portions of Downtown, but they should continue in a complimentary form and transition appropriately from one area to the next. Lighting will help establish connection as well as complement the diverse architectural styles in Downtown and Brady Village. The various components of the area lighting are intended to provide a visual link and sense of continuity for Brady Village.

- (1) **Pedestrian Lights.** Pedestrian lights are intended to be placed on lamps of six feet to sixteen feet (6' to 16') in height with an average spacing of sixty to eighty feet (60' to 80'). Actual height and spacing of lights will be based on lighting specifications and site characteristics. Lamp posts should be highly durable, low maintenance and painted in the street furniture family of colors. Refer to Exhibits 16 and 18 for “typical ornamental lamp post (pedestrian).” The pedestrian lighting system also provides an opportunity to add design elements which further links the Brady Village area internally and externally to surrounding portions of Downtown Tulsa, O.S.U. - Tulsa, and the Brady Heights historic residential neighborhood. Elements such as finials and collars can be used for festival or event displays or area wide promotional programs and activities. Refer to Exhibit 17 for typical collars and finials for ornamental lamp posts (pedestrian).

EXHIBIT 16



Typical Ornamental Lamp Post (Pedestrian)

BRADY VILLAGE
INFILL NEIGHBORHOOD DETAILED IMPLEMENTATION PLAN

Typical Ornamental Lamp Post - Pedestrian

- Pole to equal to Whatley Pole #OE411-12-BK w/ tenon as required for Holophane light fixture supplied by PSO.
- Poles shall be fluted fiberglass shaft of not less than 1/2" thickness at any point along the shaft. Shaft shall consist of 1/8" minimum thick glass filament wound fiberglass core, surrounded by polymer concrete, surrounded by exterior fiberglass urethane enamel. Poles shall be warranted for not less than 4 years from date of manufacture. Poles shall meet or exceed ANSI/NEMA C136.20 strength and wind load. Centrifugally cast fiberglass poles will not be acceptable.
- Base plate mount.
- Jim Heimdale (918)640-0784
- Fixture to equal Hadco Fixture R53BA-A1 AHRG200HE
- Architectural Lighting Sales (918)584-5554

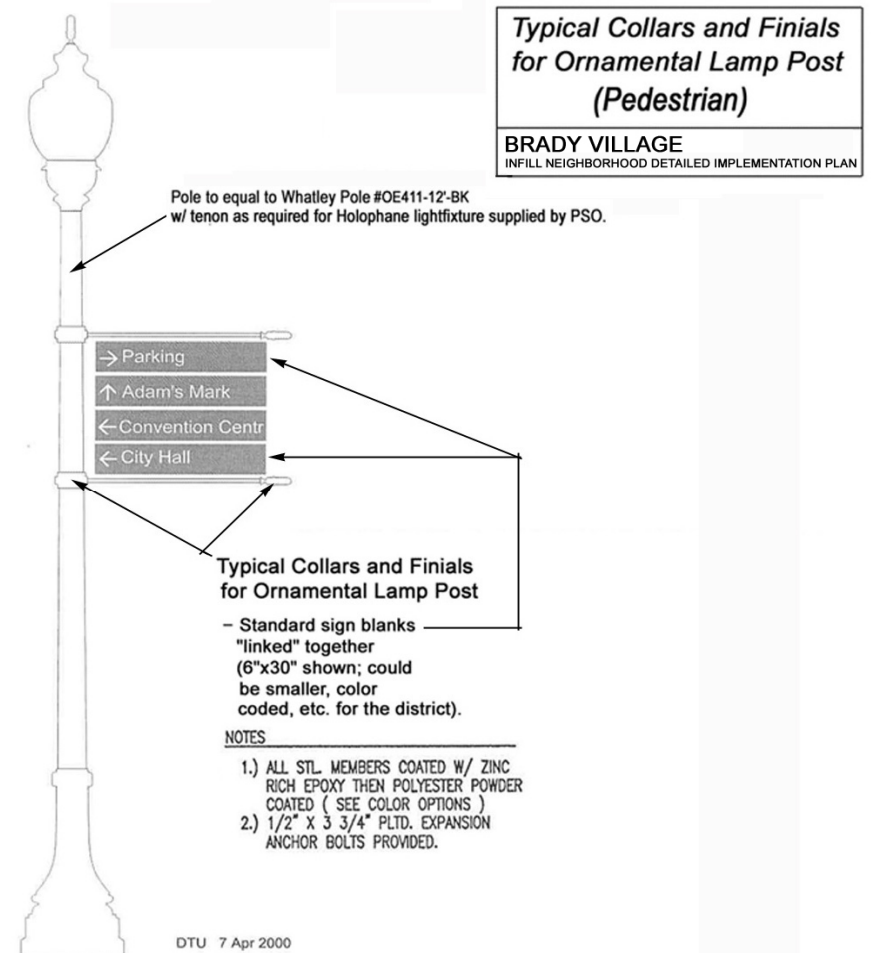
NOTES

- 1.) ALL STL. MEMBERS COATED W/ ZINC RICH EPOXY THEN POLYESTER POWDER COATED (SEE COLOR OPTIONS)
- 2.) 1/2" X 3 3/4" PLTD. EXPANSION ANCHOR BOLTS PROVIDED.

SOURCE: "Downtown Street Furniture Standards - Site Amenities", Downtown Tulsa Unlimited.
Planning and Economic Development Division, Urban Development Department, City of Tulsa.

EXHIBIT 17

EXHIBIT 18 - TYPICAL EXAMPLE OF ORNAMENTAL PEDESTRIAN LAMP POST GLOBE-FIXTURE



SOURCE: "Downtown Street Furniture Standards - Site Amenities", Downtown Tulsa Unlimited.
Planning and Economic Development Division, Urban Development Department, City of Tulsa.

- (2) **Roadway Lights.** Roadway lights are intended to be placed on lamps of sixteen feet (16') in height or higher. As a general rule, roadway lights within Brady Village should be located at every intersection and mid-block or at alleys. Actual height and spacing of lights will be based on lighting specifications and site characteristics. Lamp posts should be highly durable, low maintenance and painted in the street furniture family of colors. Replacement lamp and light fixture may be on existing roadway poles, however, new metal replacement poles may be provided and should be painted black.

The replacement roadway lamp and light fixture is to be a “white light” Holophane lamp in a “Memphis” Style fixture. Mounting may be on existing wood or metal poles. Refer to Exhibits 19 and 20 for examples of the preferred roadway lamp, light fixture and pole. Appendix N depicts the Preliminary Roadway Plan for Brady Village. Final plan recommendations will be completed in the near future.

EXHIBIT 19

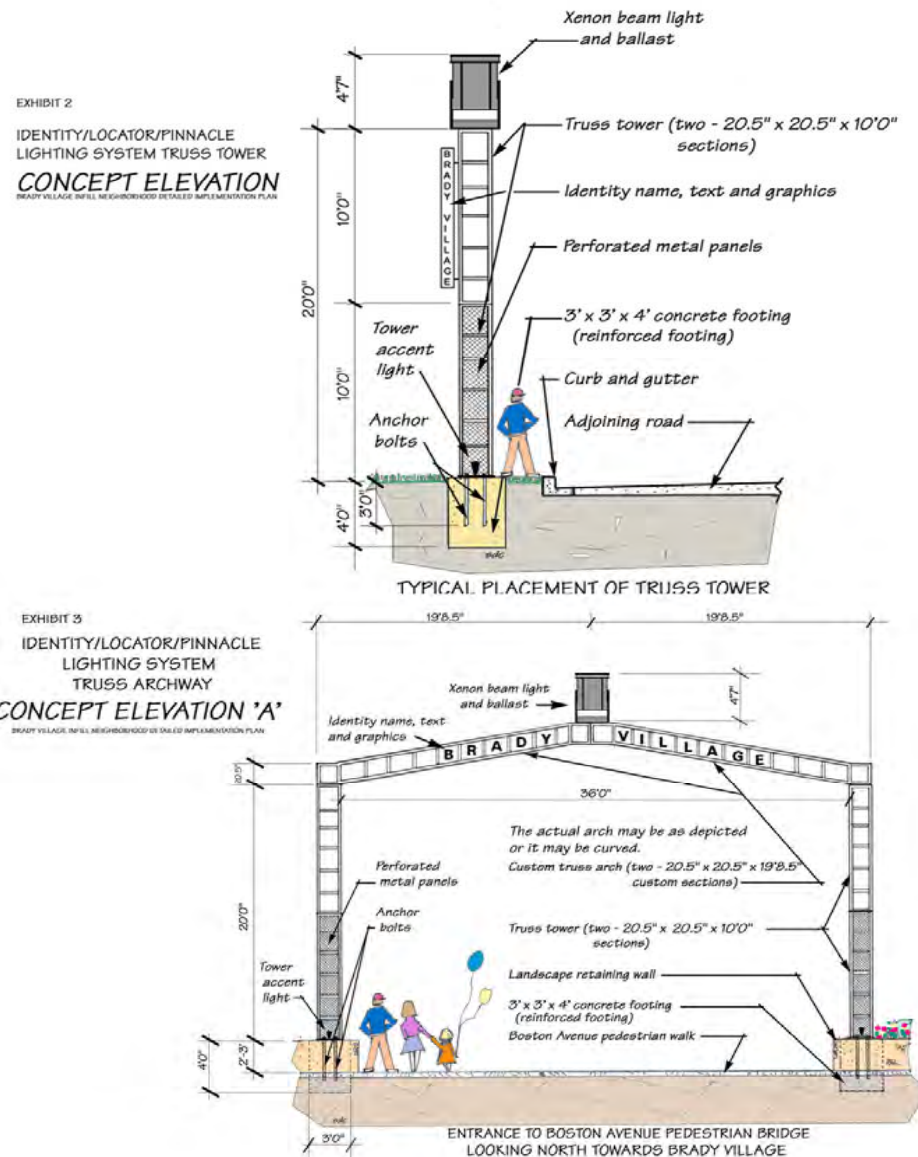


EXHIBIT 20



- (3) **Identity-Locator-Pinnacle (ILP) Light System.** The ILP Lighting System is to provide added visibility, identity and security for Brady Village. There are to be six “Xenon type” lights located around the general perimeter of Brady Village. Five of the lights are to be mounted on top of each individual towers (refer to Exhibit 21. One light is to be mounted on top of an arch located at the north end of the Boston Avenue pedestrian bridge. More specifically, the towers are to be approximately twenty feet in height of steel truss towers. The bottom 10' section of each tower should be covered on four sides by a perforated metal panel screening system to secure access. The arch will be of similar construction and design. One panel should include a lock-able, gated section to allow service and maintenance to the tower. The gated section will be designed to allow access to and reading of the individual electric meter for each tower.

EXHIBIT 21 - CONCEPT
“IDENTITY-LOCATOR-
PINNACLE” LIGHT
SYSTEM TOWERS AND
ARCHWAY



In addition, the special lighting system towers and/or archways are suggested for the “Identity-Locator-Pinnacle” (ILP) lighting system planned for the district by the Brady Village Owners’ Association. These ILP light system towers and/or archway are recommended at the following locations:

- (a) Cheyenne Avenue just north of the B.N.S.F. railroad lines;
- (b) Cheyenne Avenue and Easton Street;
- (c) Boston Avenue just south of the Inner Dispersal Loop (I-244);
- (d) Elgin Avenue just south of the Inner Dispersal Loop (I-244);
- (e) Elgin Avenue just north of the B.N.S.F. railroad lines; and
- (f) Boston Avenue pedestrian Bridge just south of Archer Street.

Other locations may be determined to be appropriate as well. A concept of the ILP light system towers and archways is depicted in Exhibit 21. It is recommended that future off-street parking lots have entry features that are similar in design and character with the ILP light system towers and/or archways. Refer to Exhibit 22 for a concept perspective.

EXHIBIT 22



*SOUTH PERSPECTIVE - "I.L.P." LIGHTING SYSTEM" ARCH AND B.O.K PARKING LOT ARCH
BOSTON AVENUE PEDESTRIAN BRIDGE*

(4) **Building Facade Lights** - Exterior building architectural facade lights are recommended for the Brady Village area to improve pedestrian and vehicle safety and visibility, and to enhance building image and area identity. It is recognized that different areas of Brady Village will likely develop different themes or primary land use, thus building architectural facade lighting themes or needs may vary. However, they should continue in a complimentary form and transition appropriately from one area to the next. Overall, a compatible design theme should be selected and followed for facade lighting that “up-lights” buildings on the perimeter of Brady Village.

E. **Signs and Graphic Art.** A common, consistent design theme and style are recommended for public identification and directional signs. This sign system will help add to a recognizable Brady Village identity. Larger identity signs, lighting features, art and/or graphics are recommended at the major entry points or gateways into Brady Village. Thematic signs, banners and special graphics are to be provided along selected portions of Brady Village Avenue streets and pedestrian-ways. Banners and graphics may also be used for promotional and festival events and activities. Informational signs are recommended to guide patrons and visitors to key attractions and additional parking.

F. **Public Art and Monuments.** It is also recommended that public art or monuments be incorporated into the Brady Village area where appropriate, but particularly at the entries into the Brady Village area. Public improvements serve many purposes which include enhancement of public image, stimulation of economic development, provision of prestige, and the recognition and of fostering cultural life. Public improvements in Brady Village, including public safety projects, should seek to achieve these same purposes. Improvements to streets, drainage facilities, and public buildings are particularly visible and should include specific features which add interest, character and identity to the area. Monument signs and bridges are examples of public improvements in Brady Village that can be provided that, with special design emphasis, can give additional character of the area. It is intended that such art and/or monuments be provided to further enhance, establish and create a unique identity and interest for the Brady Village area.

Additionally, entry arches are suggested for the bridges crossing over the railroad tracks on Boulder Avenue, Main Street, Boston Avenue (refer to I.L.P. Lighting System and Exhibit 21), Cincinnati Avenue and Detroit Avenue. The underpasses for I-244 at Cheyenne Avenue, Boulder Avenue, Main Street, Boston Avenue, Cincinnati Avenue, Detroit Avenue, and Elgin Avenue should all be repainted and properly maintained by the State of Oklahoma. These underpasses provide an excellent opportunity to provide murals that highlight the character, history, activities and events located in Brady Village. It is also suggested that special lighting be provided at these underpasses to increase pedestrian and vehicle safety, enhance the pedestrian environment, and establish attractive entries and identity for the area.

THE BRADY VILLAGE AREA

3. **Brady Village Concept Development Plan.** The Overall Design Policies and the Specific Design Policies for Brady Village are expressed in the preceding text. These Overall Design Policies and most of the Specific Design Policies are depicted conceptually in the “Concept Development Plan Venue” for the study area. This plan is presented as four “quadrant maps” for Brady Village the general areas which are shown Exhibit 23 – “Index Map of Brady Village Concept Development Plan Venues.”

The four Concept Development Plan Venues include: “Concept Development Plan Venue - Southeast Quadrant”; “Concept Development Plan Venue - Northeast Quadrant”; “Concept Development Plan Venue - Southwest Quadrant”; and “Concept Development Plan Venue - Northwest Quadrant.” These plan venues are depicted in Exhibits 24 through 27. Included on the map are those existing structures which are intended to be maintained, preserved and reused, and concept infill structures that are intended to be constructed in a manner supporting and in character with existing structures. Also depicted are on-street parking and off-street parking facilities. The off-street parking facilities are intended to be mixed use facilities that have various uses other than just parking that face area streets, particularly at street level (refer to Appendix M). Shown too are park and plaza sites, street improvements, street trees and area landscaping.

EXHIBIT 23 - INDEX MAP OF BRADY VILLAGE CONCEPT DEVELOPMENT PLAN VENUES

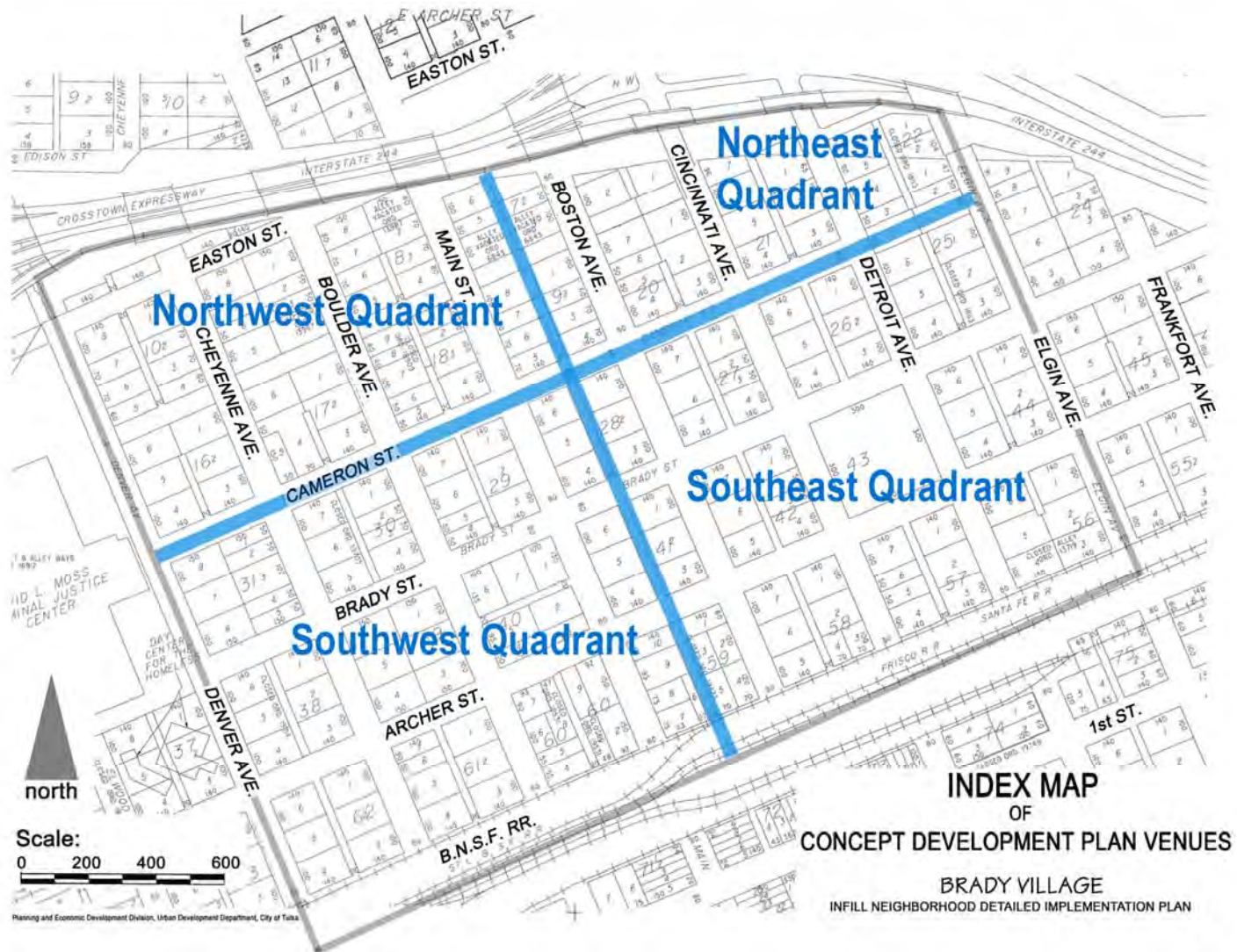


EXHIBIT 24 - CONCEPT DEVELOPMENT PLAN VENUE - SOUTHEAST QUADRANT

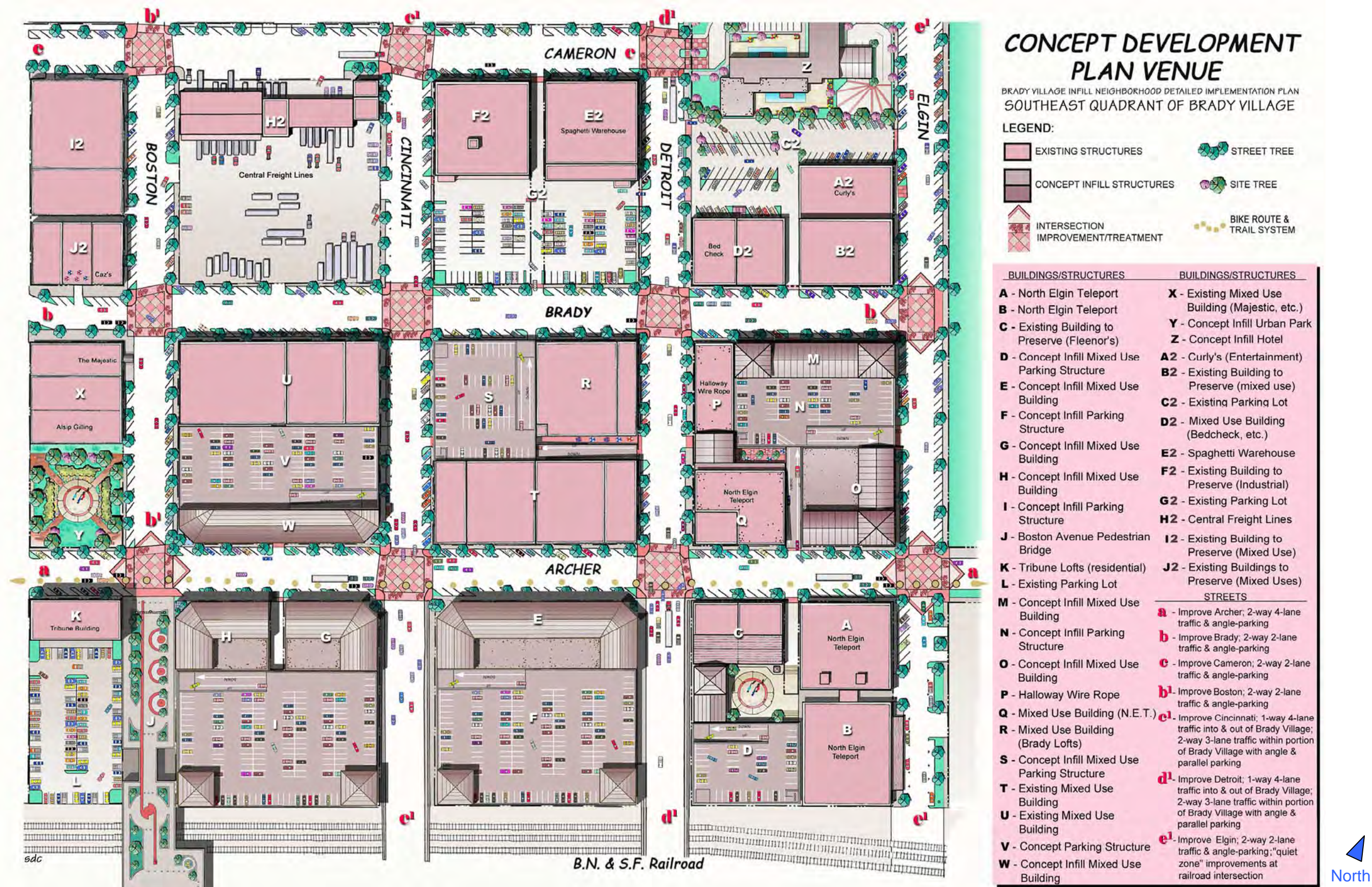


EXHIBIT 25 - CONCEPT DEVELOPMENT PLAN VENUE -NORTHEAST QUADRANT

CONCEPT DEVELOPMENT
PLAN VENUE

BRADY VILLAGE INFILL NEIGHBORHOOD DETAILED IMPLEMENTATION PLAN
NORTHEAST QUADRANT OF BRADY VILLAGE

LEGEND:

EXISTING STRUCTURES

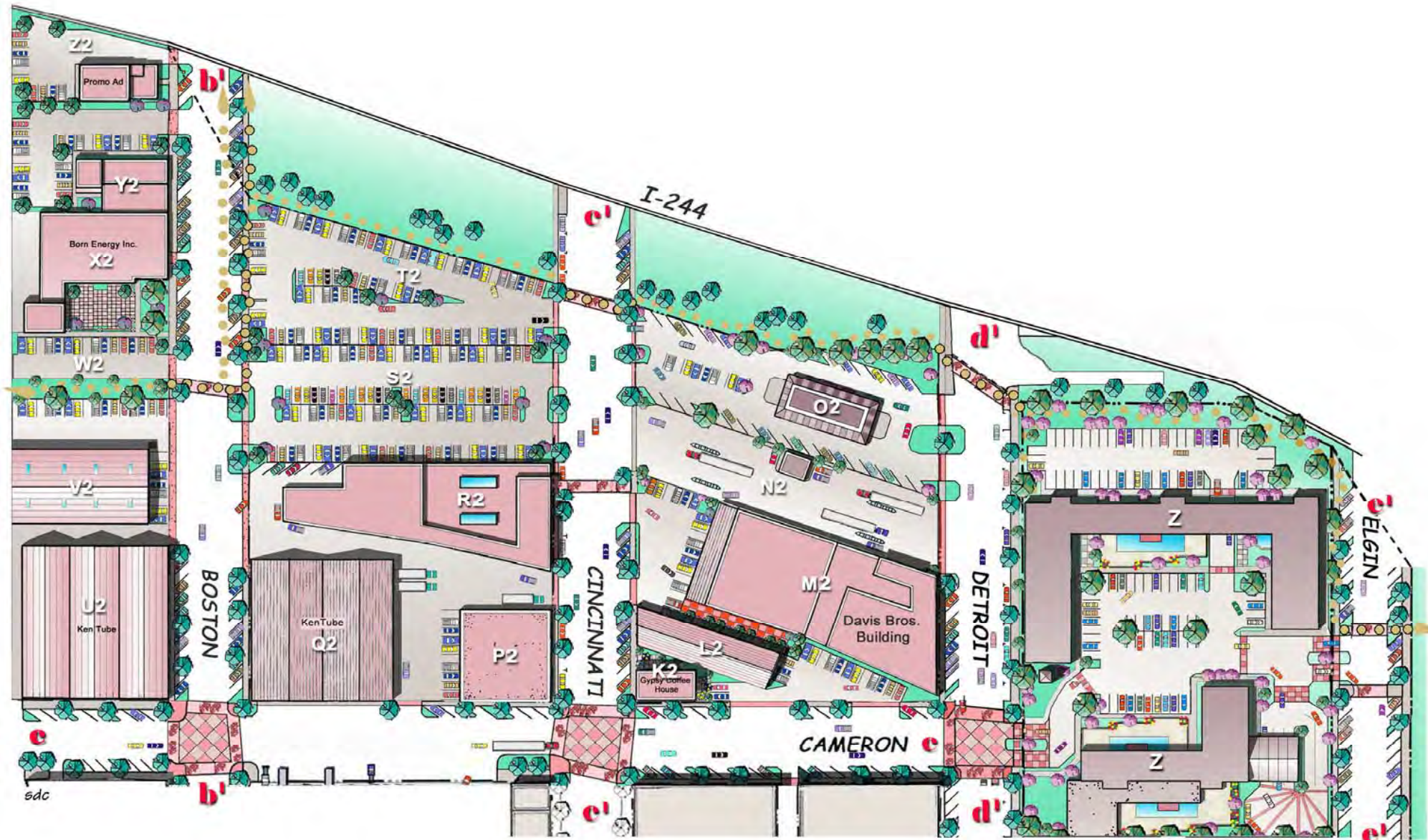
CONCEPT INFILL STRUCTURES

INTERSECTION
IMPROVEMENT/TREATMENT

STREET TREE

SITE TREE

BIKE ROUTE &
TRAIL SYSTEM



BUILDINGS/STRUCTURES	STREETS
Z - Concept Infill Hotel with Infill Parking Lots	e - Improve Cameron; 2-way 2-lane traffic & angle-parking
K2 - Existing Bldg to Preserve (Gypsy/Mixed Use)	b1 - Improve Boston; 2-way 2-lane traffic & angle-parking
L2 - Existing Building to Preserve	c1 - Improve Cincinnati; 1-way 4-lane traffic into & out of Brady Village; 2-way 3-lane traffic within portion of Brady Village with angle & parallel parking
M2 - Existing Buildings to Preserve (Davis Bros.)	d1 - Improve Detroit; 1-way 4-lane traffic into & out of Brady Village; 2-way 3-lane traffic within portion of Brady Village with angle & parallel parking
N2 - Concept Infill Fuel Station	e1 - Improve Elgin; 2-way 2-lane traffic & angle-parking
O2 - Concept Infill Fast Food/Donut	
P2 - Existing Building to Preserve (Wilson Supply)	
Q2 - Existing Buildings to Preserve (Ken Tube)	
R2 - Existing Building to Preserve (Industrial)	
S2 - Concept Infill Parking Lot	
T2 - Existing Parking Lot Expanded	
U2 - Existing Buildings to Preserve (Ken Tube)	
V2 - Existing Building to Preserve (L.A. King)	
W2 - Concept Infill Parking Lot & Building Re-use (Entertainment)	
X2 - Existing Building to Preserve (Born Energy Inc.)	
Y2 - Concept Infill Parking Lot	
Z2 - Existing Building to Preserve (Promo Ad)	



EXHIBIT 26 - CONCEPT DEVELOPMENT PLAN VENUE - SOUTHWEST QUADRANT

CONCEPT DEVELOPMENT
PLAN VENUE

BRADY VILLAGE INFILL NEIGHBORHOOD DETAILED IMPLEMENTATION PLAN
SOUTHWEST QUADRANT OF BRADY VILLAGE

LEGEND:

EXISTING STRUCTURES

CONCEPT INFILL STRUCTURES

INTERSECTION
IMPROVEMENT/TREATMENT

STREET TREE

SITE TREE

BIKE ROUTE &
TRAIL SYSTEM

BUILDINGS/STRUCTURES	BUILDINGS/STRUCTURES
A3 - Tribune Lofts (Phase 2)	G4 - Existing Building Suitable for Redevelopment
B3 - Concept Infill Parking Lot	H4 - Existing Building Suitable for Redevelopment
C3 - Williams Communication	I4 - Existing Building to Preserve
D3 - New Boulder Avenue Bridge Concept (Vehicle & Pedestrian)	J4 - Concept Infill Building & Parking Lot
E3 - Concept Infill Mixed Use	K4 - Existing Building to Preserve
F3 - Concept Infill Parking Structure	L4 - Concept Infill Development
G3 - Existing Mixed Use Buildings to Preserve (Deadtown, etc.)	M4 - Existing Buildings to Preserve (Mixed Uses)
H3 - Existing Building & Site Redevelopment Concept	N4 - Existing Buildings to Preserve (Mixed Uses)
I3 - Existing Building to Preserve (Industrial)	O4 - Existing Building & Site Redevelopment Concept
J3 - Existing Buildings to Preserve (Industrial)	P4 - Existing Buildings to Preserve
K3 - Existing Building to Preserve (Mixed Use)	Q4 - Existing Building & Site Redevelopment Concept
L3 - Existing Buildings to Preserve (Mixed Use)	R4 - Concept Infill Mixed Use Building
M3 - Existing Building to Preserve (Mixed Use)	S4 - Existing Buildings to Preserve
N3 - Existing Building & Site Redevelopment Concept	T4 - The Brady Theater
O3 - Concept Infill Parking Structure	U4 - Existing Parking Lot
P3 - Concept Infill Mixed Use Building	V4 - Concept Infill Development
Q3 - Concept Infill Parking Plaza	W4 - Meadowgold-Bordens
R3 - Concept Infill Mixed Use Building	
S3 - Existing Parking Lot	
T3 - Mexicali Border Cafe	
U3 - Existing Parking Lot	
V3 - Existing & Expanded Parking Lot	
W3 - Existing Building to Preserve (Mixed Uses)	
X3 - Concept "Brady Park & Plaza"	
Y3 - Concept Infill Mixed Use Building	
Z3 - Concept Parking Plaza	
A4 - Concept Infill Mixed Use Building	
B4 - Concept Infill Underground Parking Structure (Entry)	
C4 - Existing Building to Preserve	
D4 - Existing Buildings to Preserve	
E4 - Existing Building to Preserve or Compatible Redevelopment	
F4 - Existing Building & Parking Lot	

STREETS
a - Improve Archer; 2-way 4-lane traffic & angle-parking
b - Improve Brady; 2-way 2-lane traffic & angle-parking
c - Improve Cameron; 2-way 2-lane traffic & angle-parking
m - Improve Main; 2-way 2-lane traffic & angle-parking
b² - Improve Boulder; 2-way 2-lane traffic & angle-parking; replace Boulder Bridge
c² - Improve Cheyenne; 2-way 2-lane traffic & angle-parking; "quiet zone" improvements at railroad intersection
d² - Improve Denver as needed; maintain 2-way 4 lane traffic.

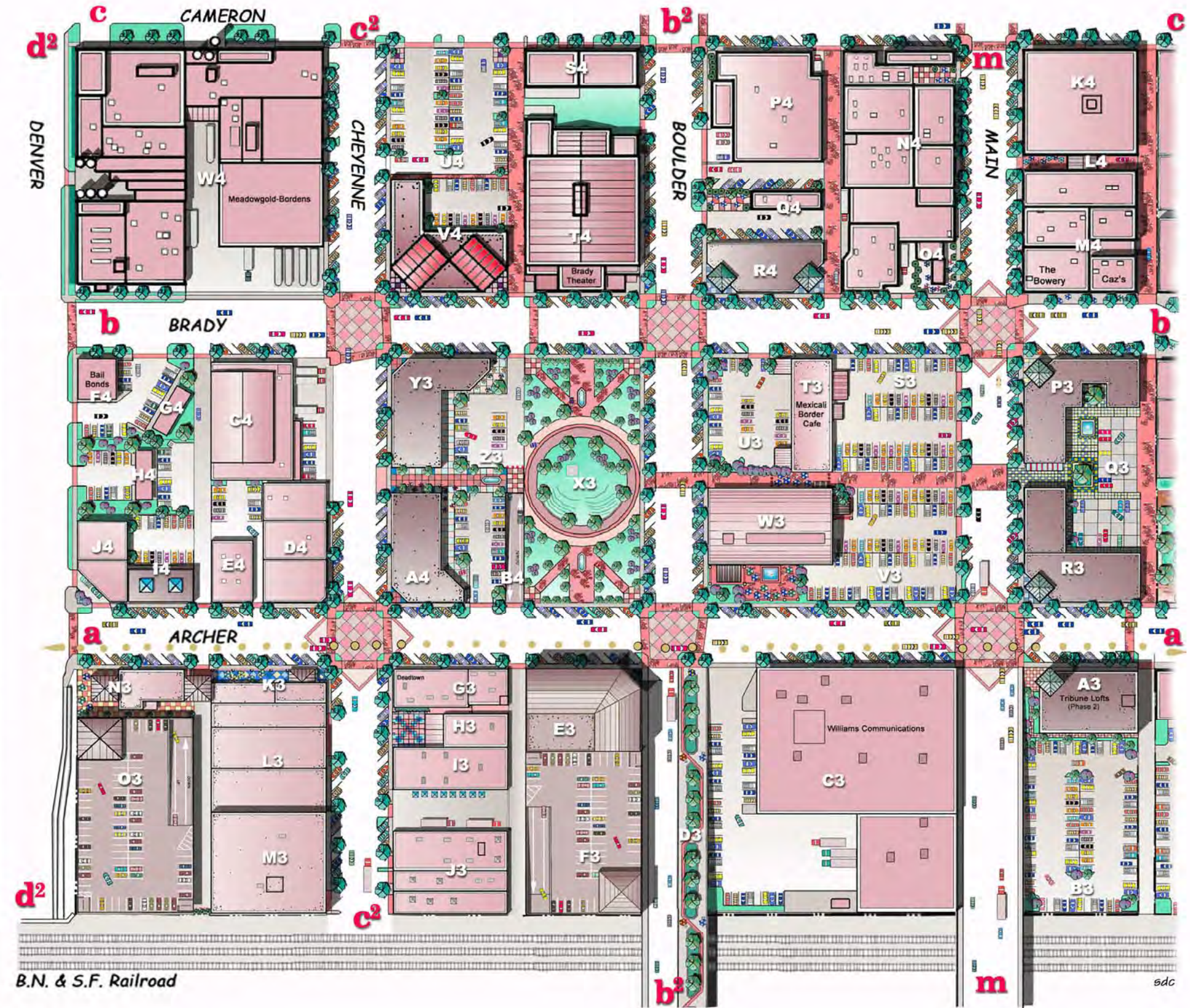
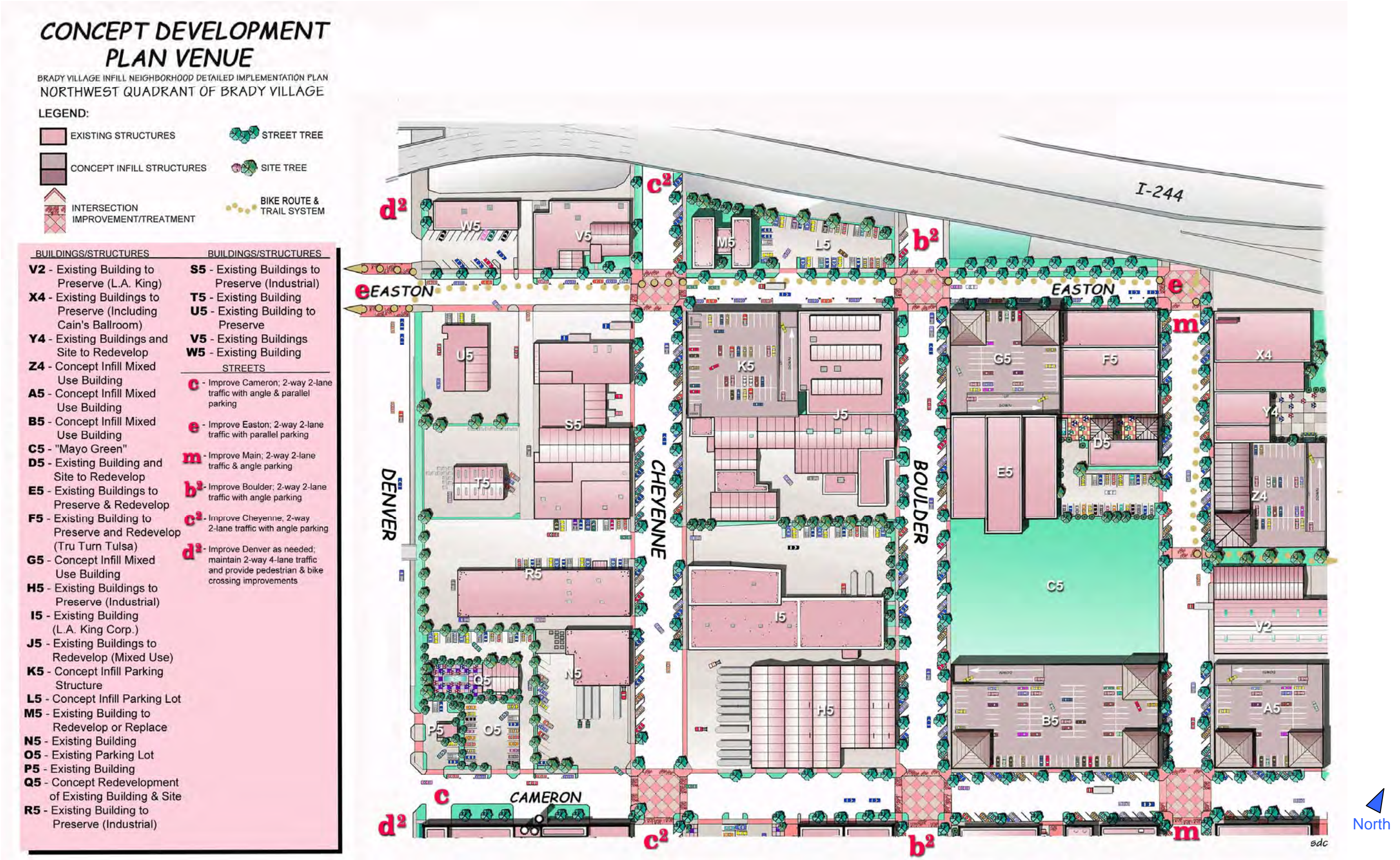


EXHIBIT 27 - CONCEPT DEVELOPMENT PLAN VENUE - NORTHWEST QUADRANT



APPENDIX A
RECOMMENDED
CAPITAL IMPROVEMENT PROJECTS
for the
BRADY VILLAGE AREA
December 19, 2000

The projects listed below have been compiled for the Brady Village area, an area bordered by I-244 (the Inner Dispersal Loop) on the north, the Burlington Northern Railroad lines on the south, Elgin Avenue on the east, and Denver Avenue on the west. The list of projects includes those that have been previously suggested for the area and those projects specifically recommended by the Brady Village T.I.F. and Property Owners Task Force. Of those listed:

- (1) Some projects will require significant capital expenditures while others will be much less expensive to accomplish;
- (2) Some projects will benefit primarily the Brady Village area, some will benefit all of Downtown, and some will be of citywide benefit;
- (3) Some projects may be publicly funded, some privately funded, and some funded through joint public-private partnerships;
- (4) Of those projects funded publicly, some may be funded from monies generated within Tax Increment District No. 1 while others will require funding from a citywide resource base; and
- (5) Some projects can be designed and implemented readily, while others will take longer to design, fund and construct.

The needs of Brady Village are great and the need for many improvements is now. It is important that the projects be prioritized in order to provide those improvements needed most as quickly as possible. The following is the list of the suggested improvements and the list is intended to be reviewed, amended and prioritized by the Brady Village task force.

The projects are as follows:

SUGGESTED SCHEDULING &/OR PRIORITY	ITEM	BUDGETARY COST ESTIMATE	POTENTIAL FUNDING SOURCES	DESCRIPTION
1	F	\$27,000 ⁱ	City funds ⁱⁱ or T.I.F. funds.	Re-striping of parking spaces and driving lanes along Main and Boulder Avenues from Archer to the Inner Dispersal Loop (I-244) along Brady Avenue from Boulder to Boston Avenues - Includes necessary street signs, reflectors, and other street improvements to provide optimum on-street parking and traffic circulation in the area.
2	A	\$75,000 ⁱⁱⁱ	City funds or T.I.F. funds ^{iv}	Fire suppression system - Fund the initial cost, for a select number of businesses, of providing the fire suppression system connection for new businesses. The system connection would consist of the water line tap, vault, valves, line and other appurtenances. It is expected that a limit on the individual amount and number of systems funded would be set.
3	E	\$75,600 ^v	City funds or T.I.F. funds.	Conversion of Cincinnati and Detroit Avenues from one-way to two-way traffic circulation in Brady Village, and provide on-street parking along Cincinnati, Detroit, and Archer - Project includes re-striping of parking spaces and driving lanes, and conversion of Cincinnati and Detroit Avenues from one-way to two-way traffic circulation from Cameron Avenue to Archer Avenue. Re-striping of parking spaces and driving lanes along Archer Street from Elgin Avenue to Denver Avenue. Includes necessary street signs, reflectors, and other street improvements to provide optimum on-street parking and traffic circulation in the area.
4	B	NA ^{vi}	Private funds and/or T.I.F. funds	Up-lighting system of select areas within the district - Lighting would be provided along Archer, Brady Streets, and portions of Cameron Streets, and along Elgin, Detroit, Cincinnati, Boston, Main, Boulder and Cheyenne Avenues.
5	H	\$297,000 ^{vii}	City funds &/or state/federal matching funds	Provide alternate bike route on Easton Street and trail system adjacent to the Inner Dispersal Loop - encourages along less traveled roadways; provides a direct connection with the existing Katy Trail System, provides more direct connection to Brady Village, O.S.U. - Tulsa, Greenwood Center, and "Katy

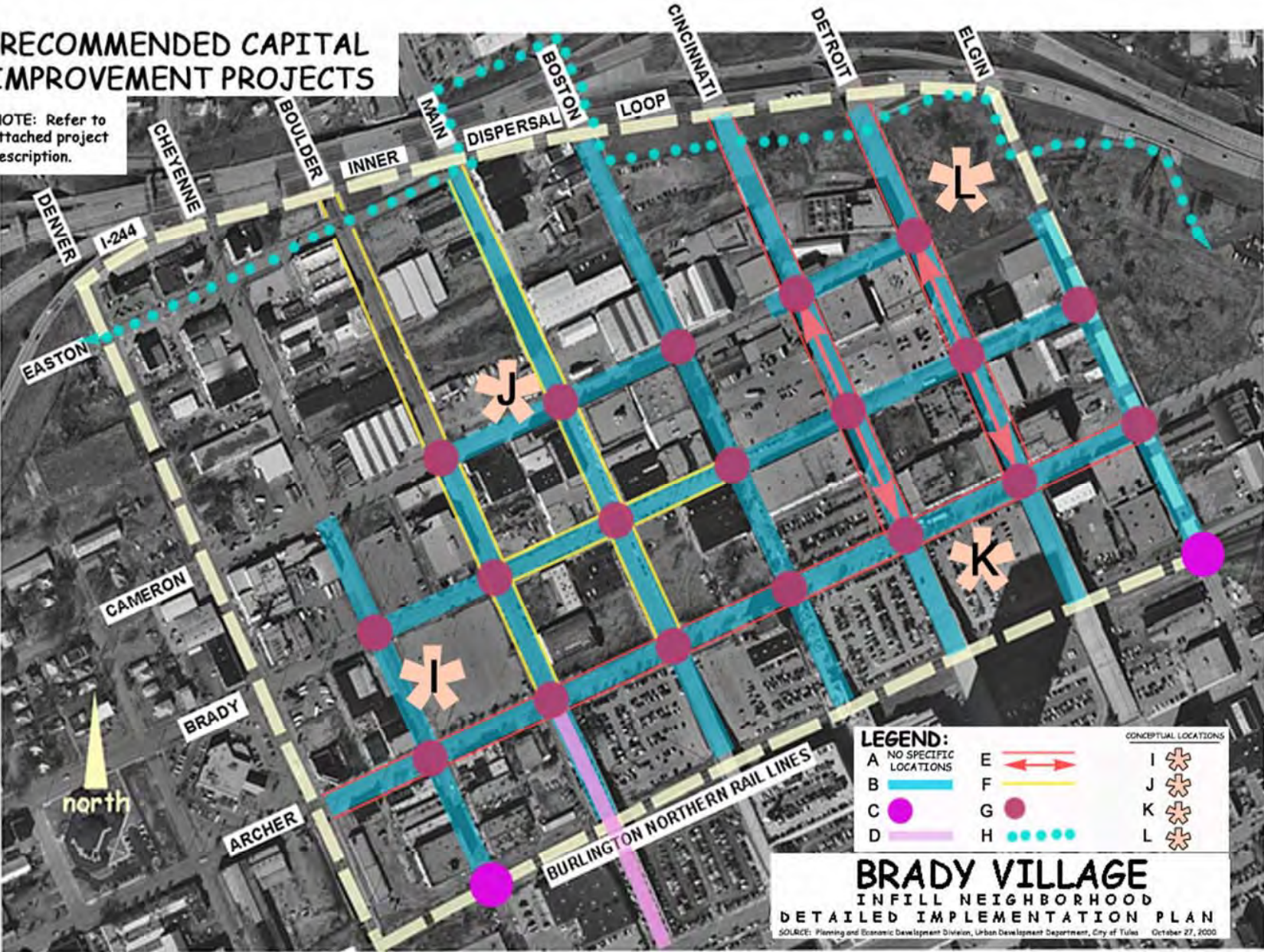
				Trail-Head Area”, and permits addition of important on-street parking along Archer Avenue.
6	C	\$1,250,000 ^{viii}	City funds, state/federal matching funds, &/or T.I.F. funds	At-grade rail line crossings guards/gates - These guards/gates would completely restrict crossing of rail lines at the streets during train use of rails. Provide at Greenwood, Elgin, and Cheyenne Avenues and First and Second Streets (the Greenwood, First and Second locations are outside of Brady Village, but are locations which impact the project area). Facilities to allow the setting of “quiet zones” in portions of Downtown, Brady Village and Greenwood Center areas for train whistles and horns.
7	G	\$2,850,000 ^{ix}	City funds &/or state/federal matching funds	Provide intersection and traffic calming improvements and install pedestrian friendly street and sidewalk features - Improvements include pedestrian “bump-outs”, sidewalk replacement and/or reconstruction, local intersection resurfacing, sidewalk access ramps, textured pavement or bricked pedestrian crosswalks, corner of intersection landscaping, streetscaping, and drainage inlets. Intersections include: Cameron & Boulder, Cameron & Main, Cameron Boston, Cameron & Cincinnati, Cameron & Detroit, Brady & Cheyenne, Brady & Boulder, Brady & Main, Brady & Boston, Brady & Cincinnati, Brady & Detroit, Brady & Elgin, Archer & Cheyenne, Archer & Boulder, Archer & Main, Archer & Boston, Archer & Cincinnati, Archer & Detroit, & Archer & Elgin.
8	D	\$6,750,000 ^x	City funds &/or state/federal matching funds	Boulder Avenue Bridge rehabilitation, repair, and renovation - Rehabilitation, repair, and renovation of the bridge over the B.N.&S.F.R.R. lines. Improvements to include structural and surfacing repair, reconstruction, and rehabilitation and lighting, pedestrian ways, and other pedestrian facilities.
9^{xi}	I	\$10,290,000 ^{xii}	City funds, private/public joint venture, &/or state/ federal matching funds	Mixed use parking facility in the southwest portion of the Brady Village District (one possible location) - Provide a mixed use facility which will include multiple-level parking, commercial, office, and residential uses. The facility will be designed to replicate the scale and design elements common to Brady Village area.

9^{xiii}	J	\$10,290,000 ^{xiv}	City funds, private/public joint venture, &/or state/ federal matching funds	Mixed use parking facility in the central portion of the Brady Village District (one possible location). Provide a mixed use facility which will include multiple-level parking, public restrooms, commercial, office, and residential uses. The facility will be designed to replicate the scale and design elements common to Brady Village area. This facility will be of particular service to the arts and entertainment portion of the district.
9^{xv}	K	\$14,350,000 ^{xvi}	City funds, private/public joint venture, &/or state/ federal matching funds	Mixed use parking facility in the southeast portion of the Brady Village District (one possible location) - Provide a mixed use facility which will include multiple-level parking, commercial, office, and residential uses. The facility will be designed to replicate the scale and design elements common to Brady Village area.
9^{xvii}	L	\$14,350,000 ^{xviii}	City funds, private/public joint venture, &/or state/ federal matching funds	Mixed use parking facility in the northeast portion of the Brady Village District and/or northwest portion of Greenwood Center area (one possible location) - Provide a mixed use facility which will include multiple-level parking, commercial, office, and residential uses. The facility will be designed to replicate the scale and design elements common to Brady Village area and the Greenwood Center area.
	TOTAL EST. COST	\$60,529,600 ^{xix}		

SOURCE: Planning and Economic Development Department, Urban Development Department, City of Tulsa, December 19, 2000.

RECOMMENDED CAPITAL IMPROVEMENT PROJECTS

NOTE: Refer to attached project description.



ENDNOTES

- i. Assumes angle-parking or parallel-parking both sides of streets, “no-parking” markings, street signs, etc.; 20,000 lineal feet @ \$0.50/LF; 100 parking and traffic signs (no parking, stop, etc.) @ \$75/each; 20 “Brady Village District” @ \$250/each; 20% contingency = \$27,000.
- ii. City funds include but are not limited to those derived from sales tax, general obligation bonds, revenue bonds, state and federal programs and other sources.
- iii. Total maximum expenditure. The maximum cost per individual “fire suppression system” is recommended to be \$15,000 and the maximum number of fire systems to fund would be 5.
- iv. City of Tulsa has made preliminary commitment to funding these projects and program with procedures and policies under preparation and review.
- v. Assumes:
 - (1) Six (6) roadway signs for two intersections and four (4) roadway signs for each of two transition zones = 20 roadway signs at \$200 each material and labor; = \$4,000; and
 - (2) Two (2) transition zones @ of 300' length each a length; low-profile and high profile pavement markers for Transition zone requiring 1 marker each 6'; 60 low-profile@ markers and 40 high-profile@ markers @ \$10 ea. material and labor; = \$1,000; and
 - (3) 4" wide, plastic coated, pavement striping and parking space marking; 5,000 lineal feet of lane striping and 24,000 parking space striping = 29,000 @ \$2/L.F. = \$58,000;
 - (4) 20% contingency.
- vi. No cost available as yet. A purely budgetary amount can be derived by multiplying 30,000 lineal feet of electrical line, lighting elements, etc. @ \$15/L.F.
- vii. Assumes 1,500 lineal feet of 10' wide, asphalt trail @ \$188/L.F. and 3,600 lineal feet of bicycle trail striping @ \$2/L.F.= \$297,200.
- viii. The typical at-grade crossing ranges from \$100,000 to \$400,000 per crossing. Five crossings at an average of \$250,000 each equals \$1,250,000. The “gate-arms” would consist of double “gate-arms” on both sides of the tracks which would totally block the roadways and totally restrict vehicular access.
- ix. Assumes \$150,000 per intersection for 19 intersections.
- x. Assumed “lump sum” total based on historical estimates increased to current costs..
- xi. Assumes that a mixed use parking structure within or immediately adjacent and serving Brady Village is a critical need. The actual location of the first parking structure to be determined based on a number of demand, logistical, and other variables.
- xii. All parking structures are designed to blend with architectural elements and features of the Brady Village District and are to be mixed use facilities. Assumes 4-story structure with 2 first floor as commercial/office; 100% top floor as residential, 2 floors (3 levels) of parking; 80 spaces/floor; 264 spaces; \$6,000 to \$12,000 per parking space, assumes \$10,000/space; plus \$100/SF for 76,500 S.F. building area.

xiii. Refer to Endnote 10.

xiv. Assumes 4-story structure with 2 first floor as commercial/office; 100% top floor as residential, 2 floors (3 levels) of parking; 80 spaces/floor; 264 spaces; \$6,000 to \$12,000 per parking space, assumes \$10,000/space; plus \$100/SF for 76,500 S.F. building area.

xv. Refer to Endnote 10.

xvi. Assumes 4-story structure with 1/4 first floor (22,500 S.F.) as commercial/office; 100% top floor as residential, 2 floors (3 levels) of parking; 180 spaces/floor for 3 floors, 130 spaces for 1st floor, and 90 spaces for 1/2 top floor parking = 760 spaces; \$6,000 to \$12,000 per parking space, assumes \$10,000/space; plus 67,500 SF @\$100/SF building area.

xvii. Refer to Endnote 10.

xviii. Assumes 4-story structure with 1/4 first floor (22,500 S.F.) as commercial/office; 100% top floor as residential, 2 floors (3 levels) of parking; 180 spaces/floor for 3 floors, 130 spaces for 1st floor, and 90 spaces for 2 top floor parking = 760 spaces; \$6,000 to \$12,000 per parking space, \$ assumes \$10,000/space; plus 67,500 SF @\$100/SF building area.

xix. Does not include a cost estimate for "up-lighting improvements" in the Brady Village District.

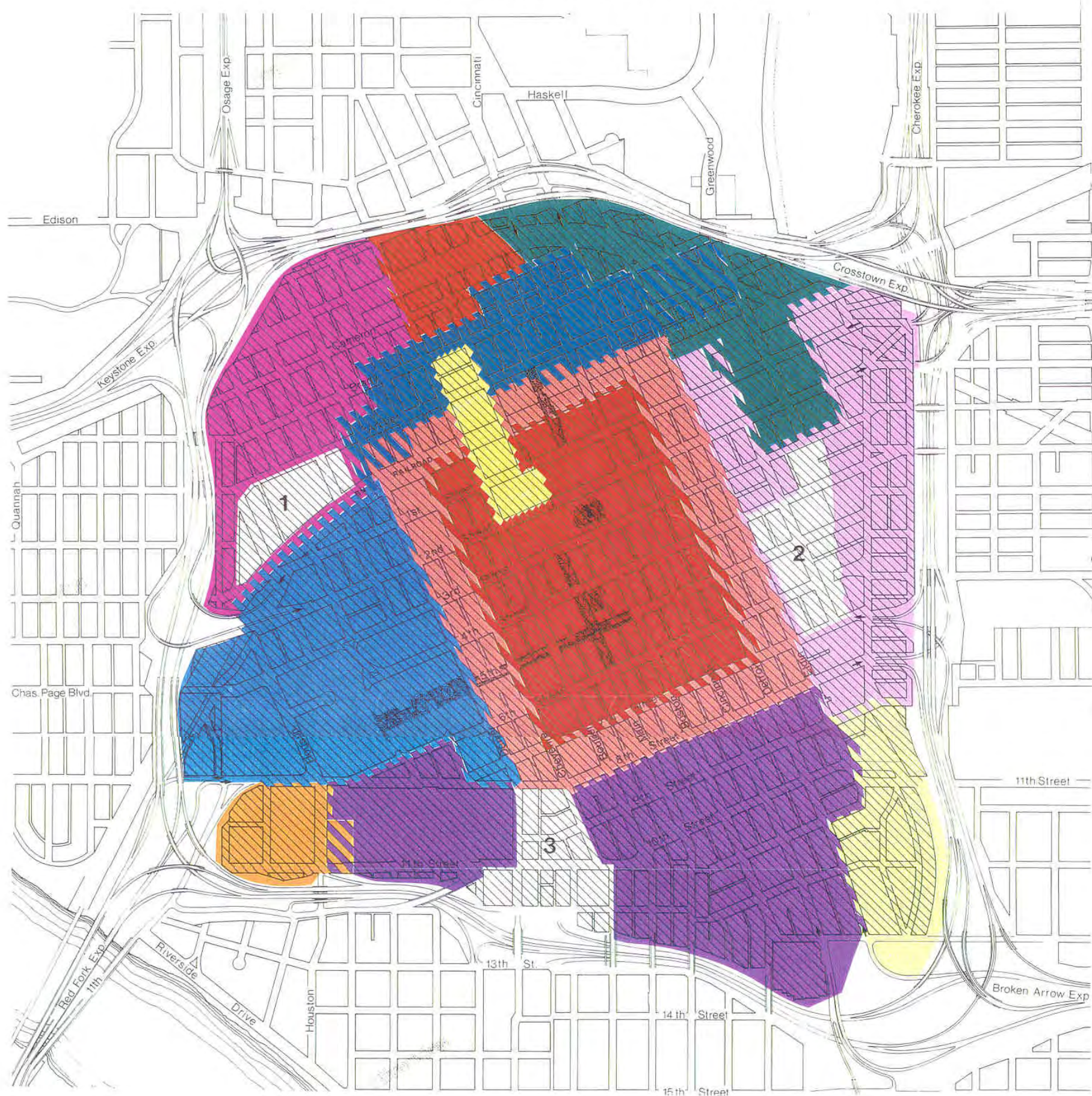
APPENDIX B

**APPROVED 2001-2006 THIRD PENNY SALES TAX EXTENSION CAPITAL IMPROVEMENT PROGRAM
PROJECTS FOR THE BRADY VILLAGE AREA**
Extension Approved May 2001

Proj. No.	PROJECT CATEGORY: Project Name	Council District	Cost	Comments
26b	BRADY VILLAGE QUIET ZONE CROSSINGS:	2,4	\$ 750,000	Installation of five ■quiet zone■ crossing gates in the area. To allow the creation of a ■quiet zone■ as identified in the Brady Plan in order to encourage development in the Brady Village area. Begins FY01-02 with \$75,000 of funding and FY03-04 with \$675,000 of funding.
26b	BRADY VILLAGE parking and streetscaping	2,4	\$ 400,000	Increase available parking for visitors and improves environment. To improve important on-street parking, addition to the trails,, pedestrian and bikeway routes through Brady Village area connecting to other existing trails to improve bicycle traffic safety by removing it from Denver and Archer to less traveled roadways (provides alternative and attractive routes). It will also provide a more direct connection to Greenwood Center, OSU and Katy Trail-head area. It also will help encourage development in the Brady Village area. Funding in FY02-03 funding of \$400,000.
TOTAL ESTIMATED COST			\$1,150,000	Estimated total cost for Third Penny Sales Tax Extension 2001-2006 projects in the Brady Village area.

SOURCE: "City of Tulsa, Oklahoma Proposed 2001-2006 Third Penny Sales Tax Extension Capital Improvements Program Projected Sources and Uses of Revenues," March 20, 2002, City of Tulsa Finance Department; "City of Tulsa Third Penny Sales Tax 2001-2006 Extension Proposal," February 1, 2001; and Planning and Economic Development Division, Urban Development Department, City of Tulsa, July 5, 2001.

APPENDIX C




THE DISTRICT 1 PLAN MAP

TULSA METROPOLITAN AREA PLANNING COMMISSION

INTENSITIES

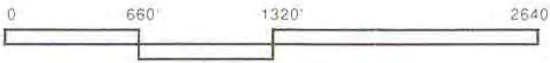
 SPECIAL DISTRICT
HIGH INTENSITY

IDENTITY AREAS

- | | |
|--|---|
|  CORE |  DOWNTOWN EAST |
|  EXPANDED CORE |  GREENWOOD DISTRICT |
|  RETAIL CENTER |  ARTS/ENTERTAINMENT DISTRICT |
|  GOVERNMENT COMPLEX |  UNIVERSITY COMMONS |
|  HOSPITAL COMPLEX |  COLISEUM AREA |
|  CATHEDRAL SQUARE |  DEVELOPMENT OPPORTUNITY SITES |
|  ELM PARK | |



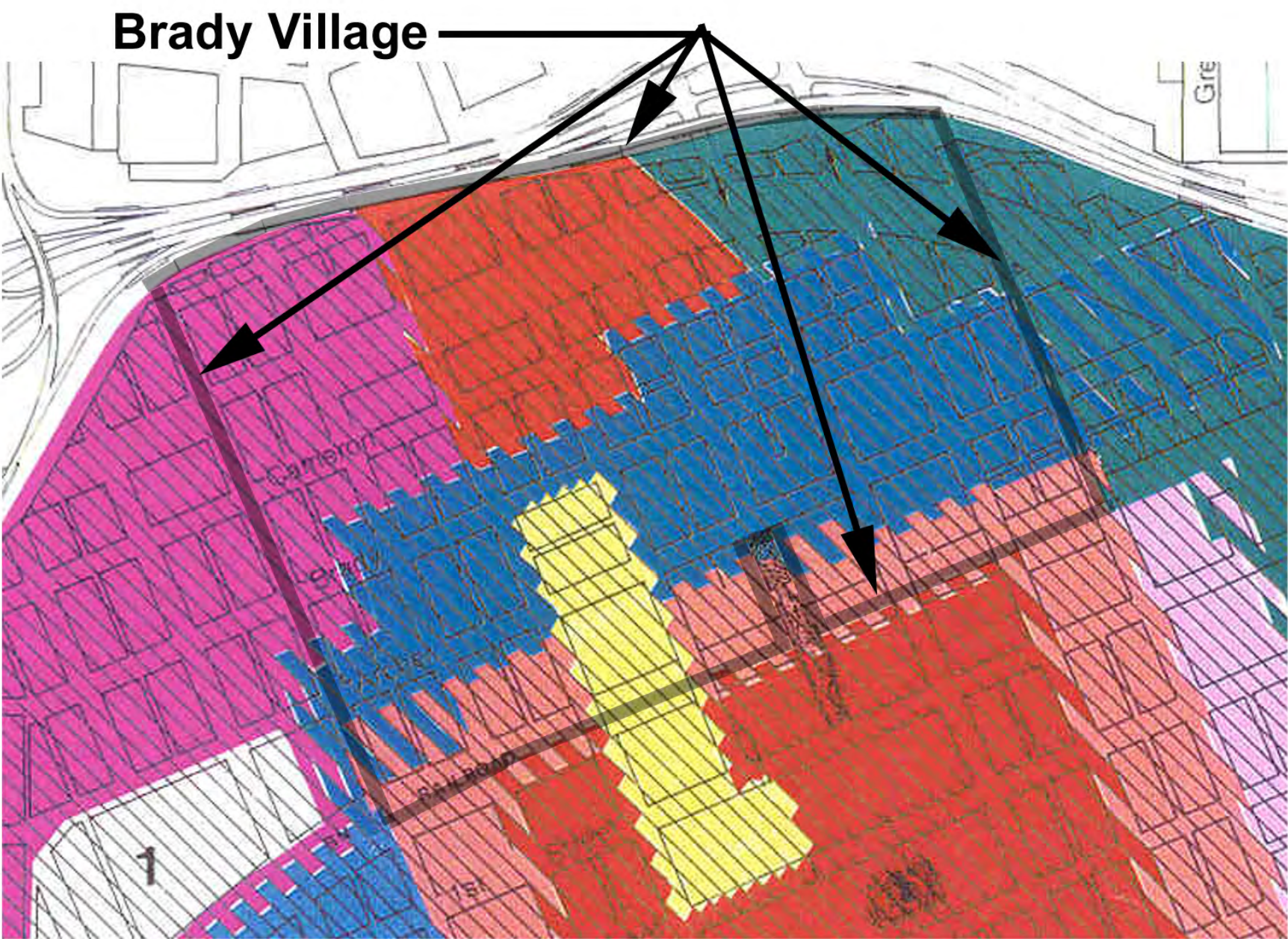
NORTH



SCALE IN MILES



APPENDIX D



THE DISTRICT 1 PLAN MAP - BRADY VILLAGE AREA

TULSA METROPOLITAN AREA PLANNING COMMISSION

INTENSITIES

SPECIAL DISTRICT
HIGH INTENSITY



IDENTITY AREAS

- | | |
|--------------------|---------------------------------|
| CORE | DOWNTOWN EAST |
| EXPANDED CORE | GREENWOOD DISTRICT |
| RETAIL CENTER | ARTS/ENTERTAINMENT DISTRICT |
| GOVERNMENT COMPLEX | UNIVERSITY COMMONS |
| HOSPITAL COMPLEX | COLISEUM AREA |
| CATHEDRAL SQUARE | 2 DEVELOPMENT OPPORTUNITY SITES |
| ELM PARK | |



BRADY VILLAGE T.I.F. No. 1 AND STUDY AREA

BOUNDARY LINE

SOURCE: Map derived from INCOG mapping for The District 1 Plan Map; Prepared by Planning and Economic Development Division, Urban Development Department, City of Tulsa, July 2002.

APPENDIX E

RECENT PHOTOGRAPHS OF BRADY VILLAGE AREA



E1 - Looking west along Archer Street from Greenwood Avenue.



E2 - Looking west-northwest towards I-244 from Detroit Avenue Bridge.



E3 - Looking northwest from Boston Avenue Pedestrian Bridge (Tribune Lofts in foreground).



E4 - Looking north along Main Street and Boston Avenue from Archer Street towards I-244 (Tribune Lofts in foreground).



E5 - Looking northeast from Brady and Boulder intersection towards Main Street and Boston Avenue underpasses of I-244 (Brady Theater in foreground).



E6 - Looking northeast at the intersections of Main Street and Boulder Avenue with Brady Street (Mexicali Border Cafe in foreground).








E7 - Looking north-northwest at Brady Street between Main Street and Boston Avenue.

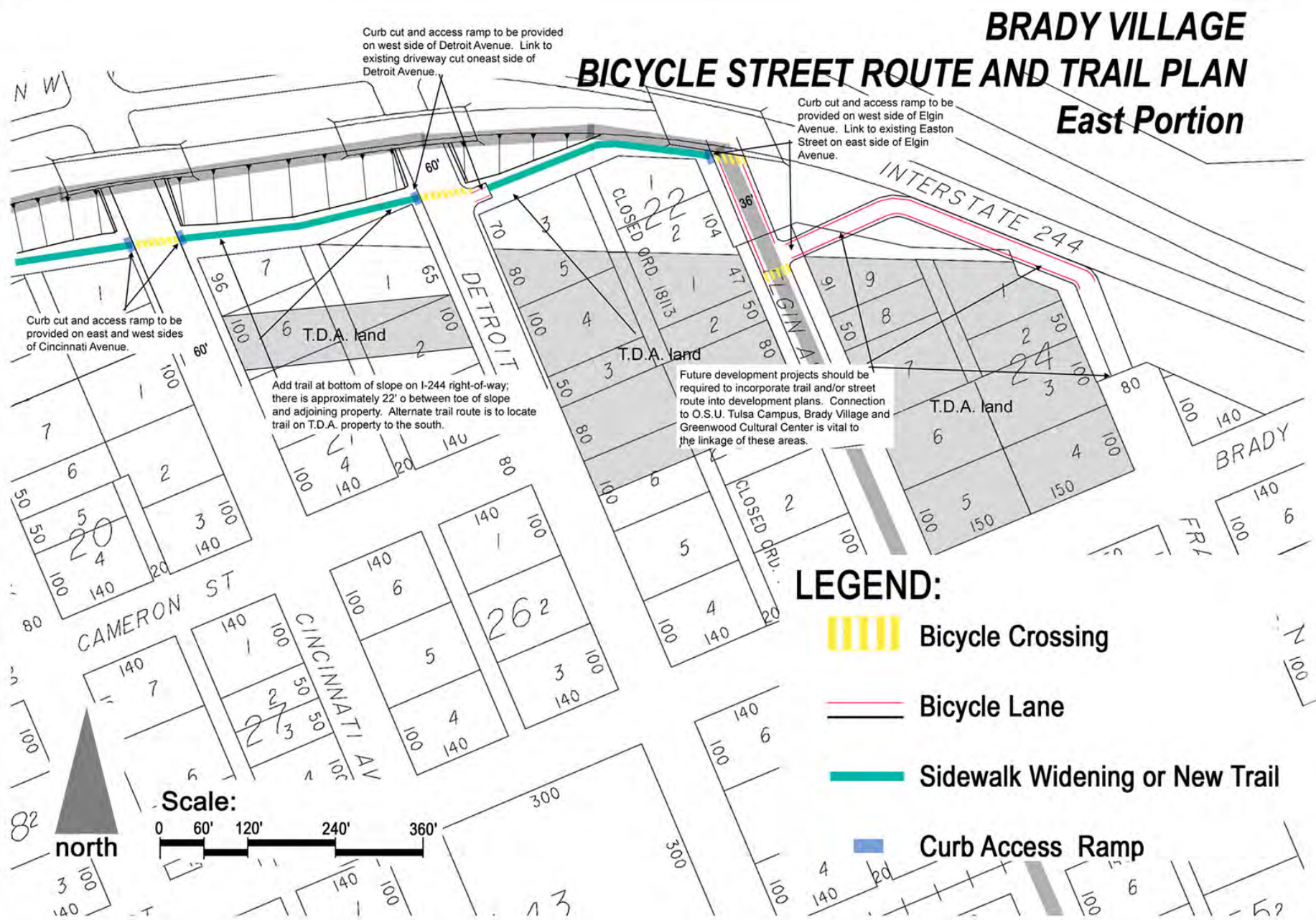
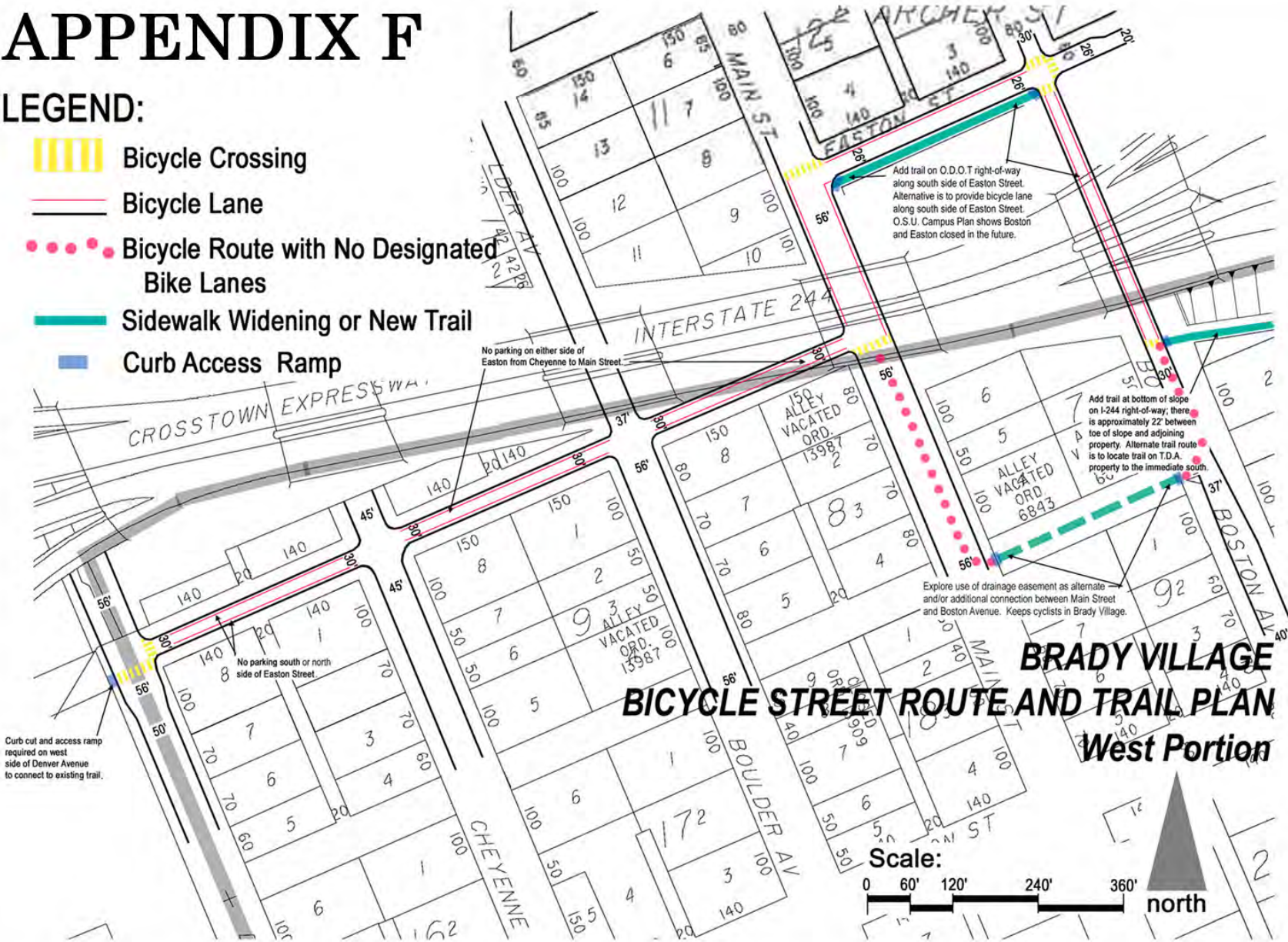


E8 - Looking southwest with intersection of Brady Street and Boston Avenue in foreground and Brady Lofts and Williams Communications in background.

APPENDIX F

LEGEND:

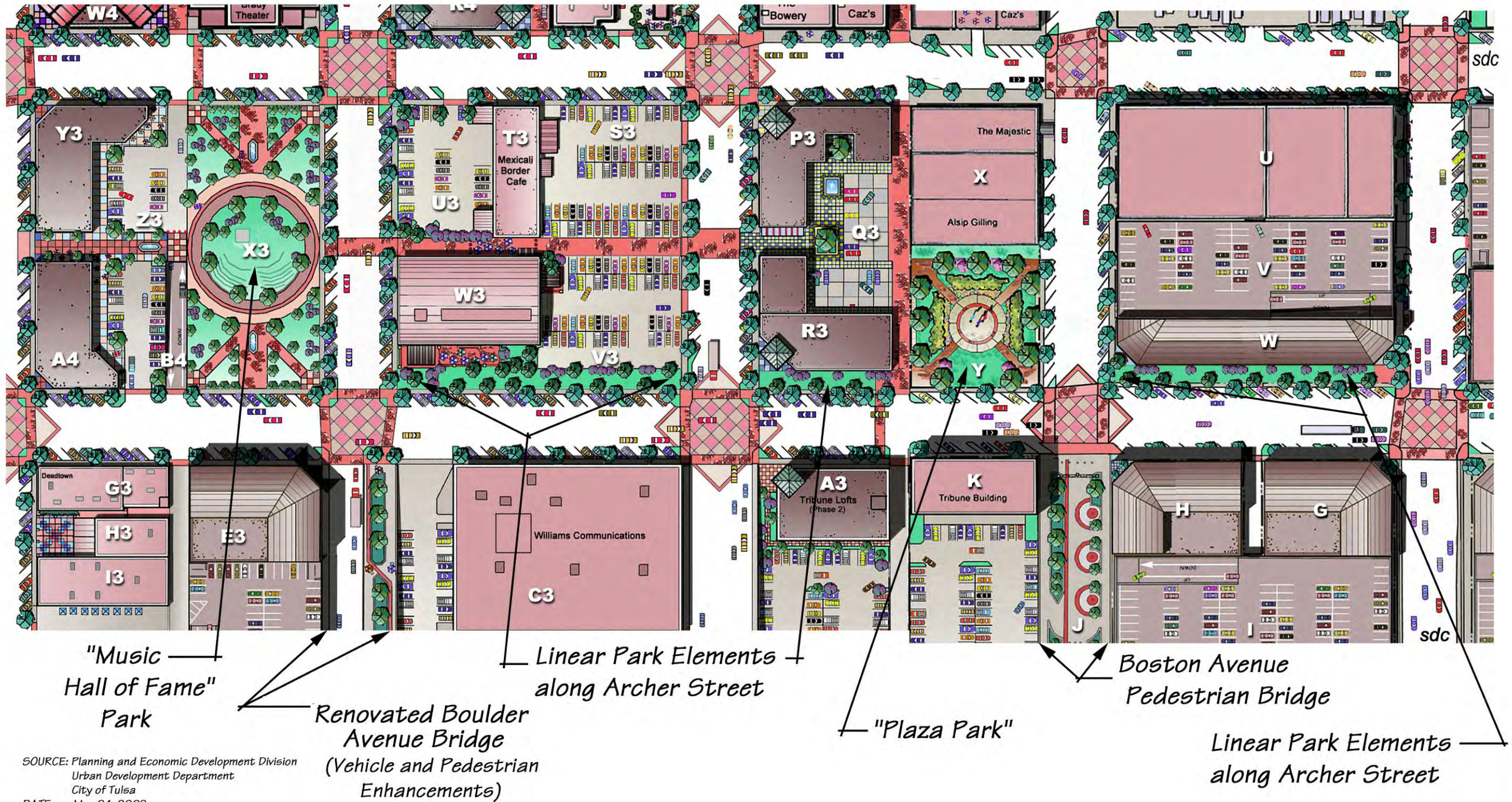
-  Bicycle Crossing
-  Bicycle Lane
-  Bicycle Route with No Designated Bike Lanes
-  Sidewalk Widening or New Trail
-  Curb Access Ramp



ARCHER STREET PARK ELEMENTS - CONCEPT A

URBAN PARK SYSTEM DOWNTOWN TULSA

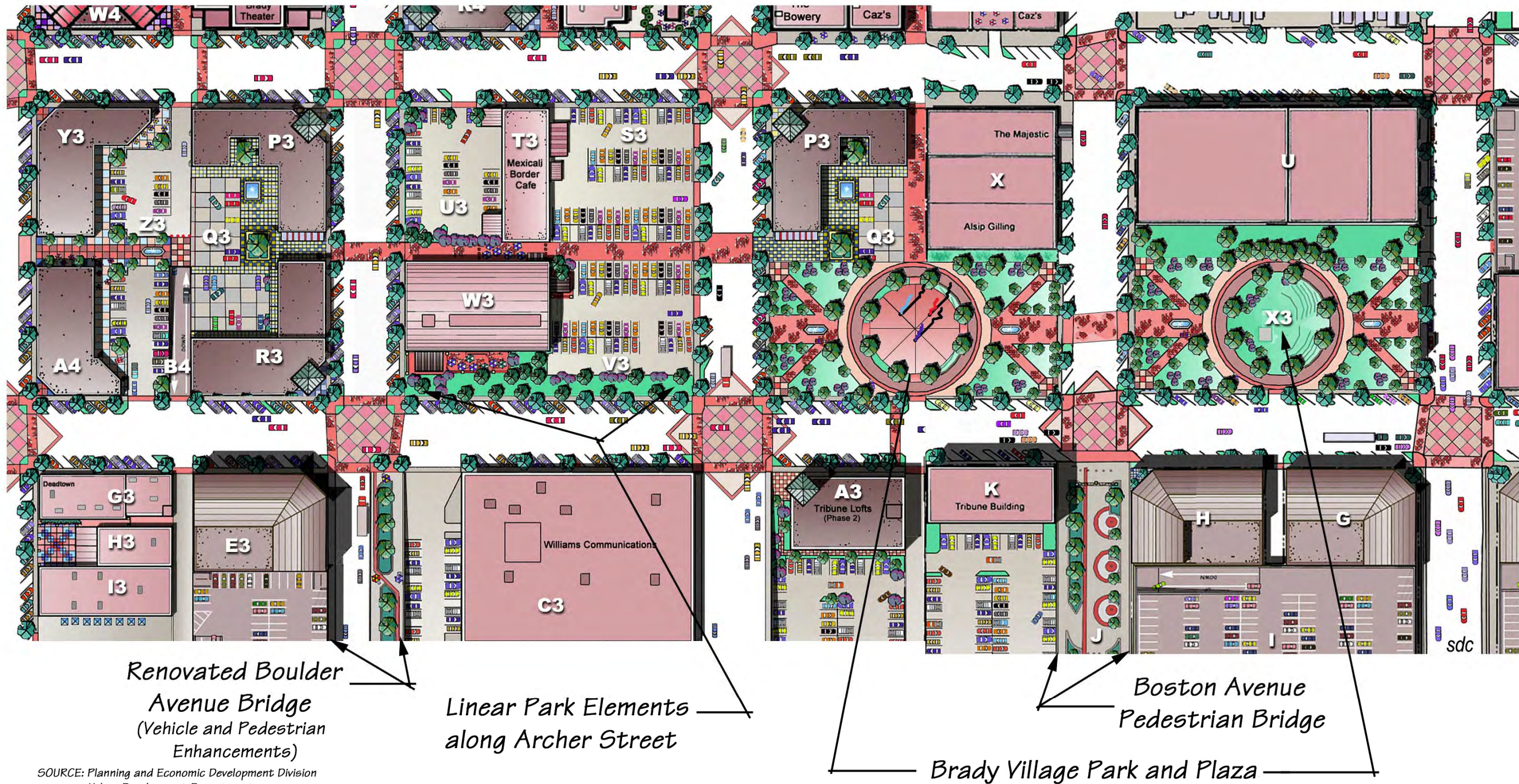
BRADY VILLAGE INFILL NEIGHBORHOOD DETAILED IMPLEMENTATION PLAN



ARCHER STREET PARK ELEMENTS - CONCEPT B

URBAN PARK SYSTEM DOWNTOWN TULSA

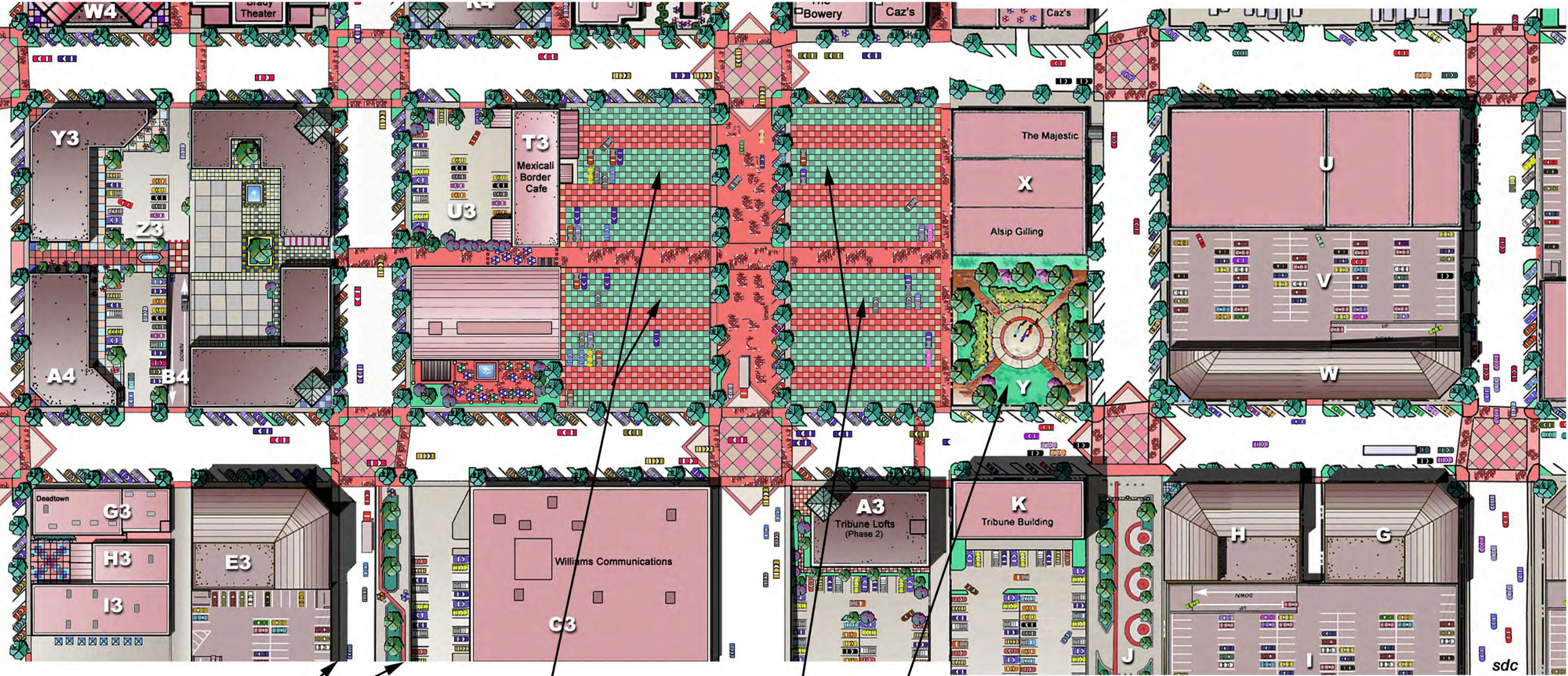
BRADY VILLAGE INFILL NEIGHBORHOOD DETAILED IMPLEMENTATION PLAN



ARCHER STREET PARK ELEMENTS - CONCEPT C

URBAN PARK SYSTEM DOWNTOWN TULSA

BRADY VILLAGE INFILL NEIGHBORHOOD DETAILED IMPLEMENTATION PLAN



Renovated Boulder Avenue Bridge
(Vehicle and Pedestrian Enhancements)

Brady Village "Courtyards and Parking Plazas"
(Festival and Events Venue; multiple use courtyards; Main Street closed to vehicles during major events)

"Plaza Park"

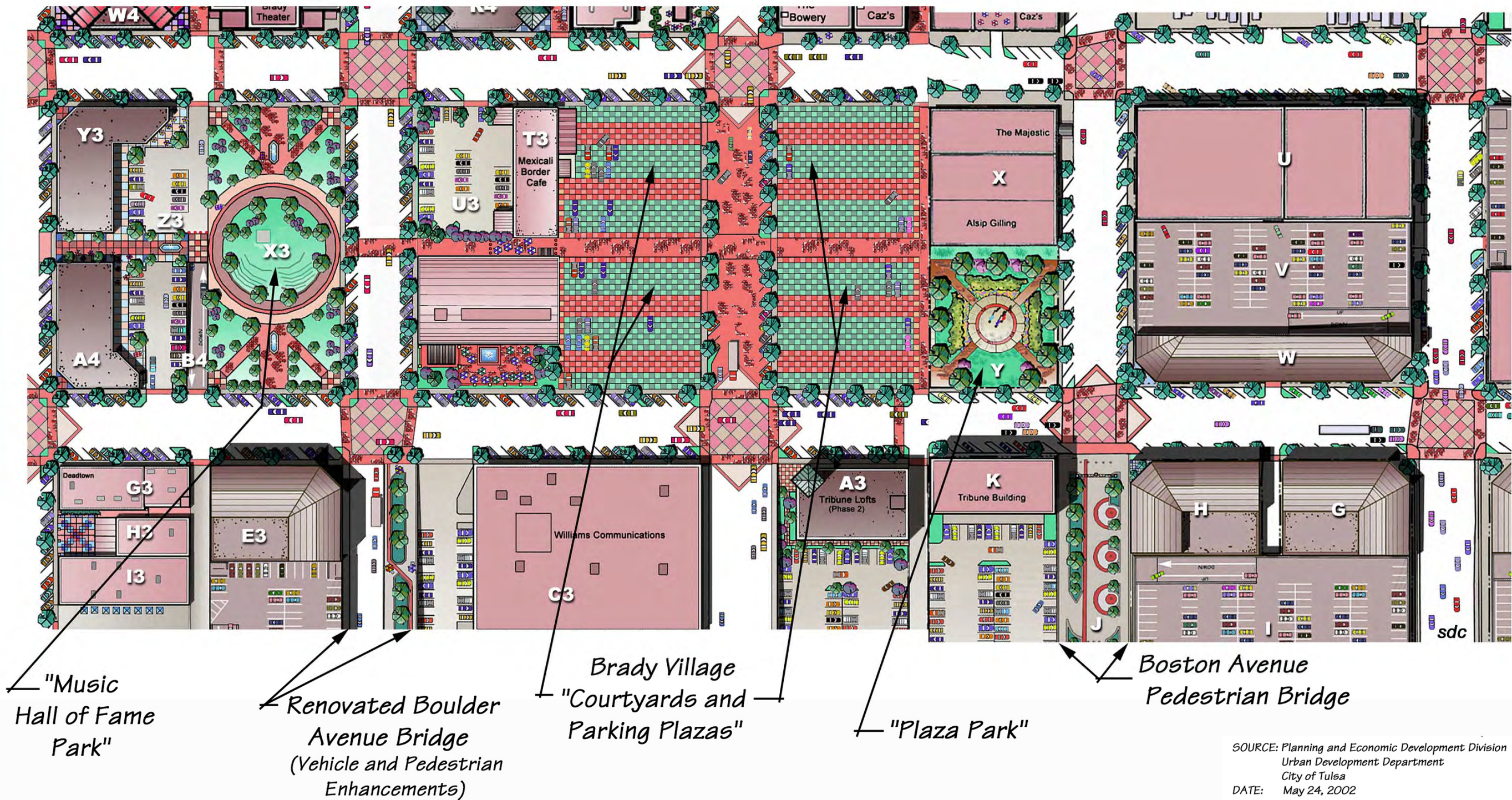
Boston Avenue Pedestrian Bridge

SOURCE: Planning and Economic Development Division
Urban Development Department
City of Tulsa
DATE: June 19, 2002

ARCHER STREET PARK ELEMENTS - CONCEPT D

URBAN PARK SYSTEM DOWNTOWN TULSA

BRADY VILLAGE INFILL NEIGHBORHOOD DETAILED IMPLEMENTATION PLAN



APPENDIX H - EXAMPLES OF INFILL DEVELOPMENT INCENTIVES AND PROJECTS

Incentives are inducements often provided by City government to encourage development within older urban or infill areas such as Brady Village. The following are examples of infill development incentives suggested for Tulsa, incentives tools used in other U.S. cities, and infill projects which used some type of incentive to facilitate development or redevelopment.

EXAMPLES OF SUGGESTED INFILL INCENTIVES FOR TULSA DERIVED FROM TULSA INFILL DEVELOPMENT TASK FORCE, BRADY VILLAGE TASK FORCE AND BRADY VILLAGE INFILL NEIGHBORHOOD DETAILED IMPLEMENTATION PLAN STUDY

- ✧ ***Develop faster permit processing and approvals.*** Assign an “Infill Development Facilitator” or ombudsman to areas designated as Special Districts in the Tulsa Comprehensive Plan. The Facilitator would serve as an initial point of contact for projects within Special Districts and/or infill areas, and assist in processing reviews, permitting and approvals of infill projects in these areas. The Facilitator would be responsible for bringing together special development and building permit review teams whose work would insure an enhanced process in terms of the time required for review and approval and decision making on important issues. Permit reviews, inspections and other development decisions should be made a priority within these special areas and be guaranteed faster response times than normal. The enhanced process could include such processes as: building permit, building inspection, construction inspection, certificate of occupancy, zoning, platting, Board of Adjustment, privately funded public improvements (pfpi), right-of-way access and other regulatory matters affecting development and construction. The Facilitators role would be to insure “speedy and priority treatment” and facilitate communication and understanding of project plans, elements and potential impacts on the surrounding area.
- ✧ ***Develop and adopt changes to a Existing Building Codes for the City of Tulsa.*** These building codes should allow and facilitate the development and redevelopment of the building stock in older parts of Tulsa and encourage within these urban areas the mixed use of buildings, including the use of second and third floors as commercial, office or residential space. The City of Tulsa adopted a new existing building code in 2001. The purpose is to make the repair, remodeling, reconstruction and adaptive reuse of existing buildings within Brady Village and other areas of the City easier and more predictable for the development, construction, architectural and design community. The new existing building code will in effect help facilitate the conversion of existing buildings to mixed use, including commercial, office and/or residential space.

- ✕ ***Expand the City's Fire Suppression Grant Program into areas similar to Brady Village.*** Presently the City offers a fire suppression grant program to property owners within the three existing tax increment districts who wish to install fire suppression systems in their new or refurbished buildings. The grant pays half the costs, or up to \$8,000, for the installation of fire suppression vaults, the assembly within the vault, the water tap at the main and other components which are necessary to provide appropriate water service for the fire suppression system. The piping on the customer's side of the vault is not included. This program should be expanded to include areas that are designated as Special Districts within the Comprehensive Plan for the Tulsa Metropolitan Area or within special Infill Areas.

- ✕ ***Develop a reduced fee schedule for City regulatory permits and processes applicable within Special Districts and infill areas such as Brady Village.*** Develop a reduced fee schedule for development, building, construction and other permits, zoning and board of adjustment applications and other City regulatory processes that is lower in Special Districts and/or Infill Areas than is applicable in other parts of the City. The lower fee schedules, coupled with enhanced review and approval processes, provide an economic incentive to developers and builders for constructing projects within the Special Districts and Infill Areas of the City.

- ✕ ***Concentrate available City financial resources to fund public improvements within the Brady Village area.*** Needed public infrastructure projects such as street improvements, storm water drainage, replacement water lines and other capital improvements should continue to be proposed for funding in upcoming 3rd penny sales tax extensions, bond issues, and the City's Capital Improvement Program (CIP). The infrastructure needs within Special Districts or in special infill study areas should be given top priority in the City's CIP and in future proposed public funding packages. Public funds and improvement efforts should be focused on these important urban areas to provide highly visible improvements and development to serve as catalysts for additional redevelopment efforts in core urban areas.

- ✕ ***Utilize available economic development tools within the Brady Village, Special Districts and other Infill Areas.*** Use where appropriate available economic development tools to foster redevelopment in these special areas. These tools include: tax increment financing, tax incentive districts (already in use in Brady Village), improvement districts, special economic development funds (such as the 1985 Sales Tax Economic Development Fund), community development block grant, historic tax credits and other techniques. Their purpose is to assist in funding public and private improvements within the Brady Village area. Utilize these economic development tools in determining how to address identified needs within the Brady Village neighborhood or in structuring innovative public/private partnerships in conjunction with private redevelopment efforts within the Brady Village special infill area. These economic development tools may assist in making a private redevelopment effort become financially feasible where without these tools the proposed project would remain financially infeasible. The tools may also assist in funding needed public improvements

within the area (public parking, streetscaping, buffers, entries, and public art) when other public funding sources are not readily available.

- ✕ ***Utilize the resources and abilities of City Authorities/Commissions to help achieve the Brady Village vision and goals.*** Utilize the resources and abilities of the various City Authorities/Commissions in accomplishing various projects within the Brady Village area. An example might include involving the Tulsa Parking Authority in planning, designing, financing, and constructing public parking facilities within the Brady Village business areas if determined necessary and appropriate. Another example might involve the Tulsa Preservation Commission in neighborhood issues involving preservation or the use of historic tax credits. Perhaps another example might be the task of the Tulsa Development Authority acquiring specific blighted sites for infill development or redevelopment. City authorities and commissions determine that addressing the special needs of older urban areas of Tulsa is a major priority. Special assistance will be given to assisting development and redevelopment efforts to modern standards while preserving, maintaining and enhancing the unique characteristics that make such areas special and attractive.

EXAMPLES OF INFILL DEVELOPMENT INCENTIVES IN U.S. CITIES

The following are examples of infill development incentives that have been discussed and/or have been tried in cities in the United States. Each represents an expressed desire or an actual attempt to encourage and facilitate quality development in older parts of these cities. Such techniques might be suitable for consideration in our community, or these techniques may stimulate other practical, innovative ideas. The examples are as follows:

- ❑ **GENERAL REFERENCE TO INCENTIVES - Greene County Hopes to Lead the Way in Conservation Development** - Greene County, Maryland offers *incentives* to developers that build cluster housing on smaller lots while providing in some instances conservation easements designed to improve views of local residents and leaving tracts of land undeveloped for open space. (Source: ULI web site, 06/01/2001).
- ❑ **PROPERTY TAX INCENTIVES, FASTER PERMIT APPROVALS AND PRIVATE-PUBLIC PARTNERSHIPS - Towns Using Law to Ease Redevelopment** - A number of South New Jersey towns are taking advantage of a 1992 Local Redevelopment and Housing Act to encourage more redevelopment and private investment activity by offering *property tax incentives, faster permit approvals, and public-private partnerships*. (Source: ULI web site, 06/01/2001).
- ❑ **FINANCIAL INCENTIVE - Rebuilding East Bradenton; Federal Redevelopment Plan Should Spur City** - A *federal grant* is being used to raise and rebuild a federally subsidized apartment building in a blighted area of Bradenton, Florida. Local officials are hoping to create a special taxing district in the area to generate additional new revenue the city could offer as *incentives for developers and homeowners* interested in rebuilding in the area. (Source: ULI web site, 06/01/2001).
- ❑ **HOUSING BONUSES, TRANSFERABLE DEVELOPMENT RIGHTS AND FEE WAIVERS - The Changing Landscape of a Growing Eastside City** - The City of Redmond, Washington is encouraging more residential development in its downtown area. Trammell Crow Co. and Intracorp Real Estate LLC are preparing mixed-use projects to capitalize on recent job growth. The city is establishing housing initiatives and providing incentives for developers. *Incentives may include housing bonuses, expansion of transferable development rights, and certain (impact) fee waivers*. Some neighborhoods have also been *rezoned to accommodate residential development*. (Source: ULI web site, 06/01/2001).
- ❑ **PERMIT CLUSTERING OF HOMES - Density, Traffic, Open Space Lead “Smart Growth” Issues** - The U.S. Department of Agriculture has found that stopping suburban growth is not seen as possible. Strategies are being proposed to mitigate problems and foster solutions. Lobbying efforts for smart growth have centered on housing density, improved transportation and preservation of open space. One strategy being suggested includes *clustering homes in a more compact segment of*

land and preserving the balance as open space. The higher densities make stores and transit stops more likely. Residents and public officials often fear more congestion, reduced property values, and ushering in of low-income residents. (Source: ULI web site, 06/01/2001).

- ❑ **GENERAL REFERENCE TO INCENTIVES - Helping Cities Grow Smartly** - Continued sprawl is in part due to a strong economy. Smart growth debate centers on reducing air pollution and traffic congestion, protecting open space, and preserving quality of life in fast-growing metropolitan areas. Must also find an approach that does not trample on the rights of property owners. Portland, Oregon is a model of growth control that has also seen its efforts boost the cost of homes. Realtors have taken the stance to not completely stop growth nor to disallow growth restrictions but rather to *offer incentives for developers that keep community concerns in mind when planning and building new projects.* (Source: ULI web site, 06/01/2001).
- ❑ **GOVERNMENT GRANTS, TAX RELIEF AND REDUCED PARKING REQUIREMENTS - Lawmakers Form Smart Growth Caucus to Try to Limit Urban Sprawl** - California state legislation has been proposed that would require (among other measures) the state to offer *government grants, tax relief and other incentives to convert old commercial buildings into new housing; special tax credit for people donating land to be used as open space; and require local governments to permit denser development projects with less parking if located closer (e.g. - 1/8th mile) of transit facilities.* (Source: ULI web site, 06/01/2001).
- ❑ **TAX BREAKS AND REGULATORY RELIEF - U.S. House Backs Incentives** - *Tax breaks and other incentives* will be offered for 10 years to encourage investments and new housing projects in neighborhoods designated as 40 renewal zones. Developers and /or businesses may take advantage of *regulatory relief, environmental assistance, home ownership incentives, and tax breaks* in these areas. (Source: ULI web site, 06/01/2001).
- ❑ **FINANCIAL INCENTIVES - Austin Gives Breaks to Protect Its Hills** - Austin is to offer the Vignette Corp. nearly \$25 million in incentives to draw the company deeper into town. (Source: ULI web site, 06/01/2001).
- ❑ **TAX INCENTIVES - Searching for Space, Cities Go Vertical** - The world's tallest skyscraper has been proposed on 65 acres on once polluted downtown property. The City of Dallas offers tax incentives for homeowners to migrate downtown *and financing to companies* to come in and revamp older properties, and has set aside public areas for beautification. (Source: ULI web site, 06/01/2001).

- ❑ **SPECIAL REGULATIONS - Brookside Neighborhood, Kansas City, Missouri** - Kansas City Council voted to approval of two ordinances that are intended to protect the Brookside neighborhood from large-scale, suburban style chain store development, and to protect and maintain small-scale, neighborhood serving businesses and pedestrian nature. One ordinance creates the Brookside Business District as a special zoning district with its own rules and requirements. The district encompasses a mix of residential, office and commercial uses. The second ordinance restricts retail uses to an area of 10,000 square feet except for grocery stores which may be 25,000 square feet. Auto-dependent businesses serving a larger geographical area are specified as inappropriate for the district. (Source: “New Rules: Retail: Size Caps: Brookside”, News & Bulletins; uli@newrules.org, May 2001).
- ❑ **TAX RELIEF, DENSITY BONUSES, REGULATORY RELIEF AND INFRASTRUCTURE IMPROVEMENTS - Truly Smart Growth** ■ ULI chairman says that for American cities to enjoy lasting smart growth success, “affordable housing” must be included along with terms such as “pedestrian-oriented”, “mixed use”, and “transportation options”. Do not “out price” existing residents. ULI defines smart growth as “environmentally sensitive, economically sound development that supports community livability and permanence”. Others stated *that smart growth includes incentives to promote investment, lowering regulatory barriers, and using public funds to improve infrastructure*. It is important that as area gentrification occurs that *existing housing residents and local small business owners are not driven out*. Create a “sense of community”. Link all aspects of neighborhood and community so that they all have a fair share of the benefits. Retaining existing residents of a neighborhood can provide for an integrated social network, purchasing power to sustain retail development, and a concentrated work force. *Incentives* that help spur development include: *affordable housing, local tax relief, density increases for affordable housing, and inclusive zoning law* that allows for a wide range of uses. (Source: ULI web site, 06/01/2001).
- ❑ **FINANCIAL INCENTIVES AND TAX INCENTIVES - Pittsburgh Cultural District, Pittsburgh, PA** - The Pittsburgh Cultural Trust is a nonprofit trust created by the combined leadership of civic, corporate and municipal efforts. The Trust began renovating, supported by local philanthropists, several old theaters. Over 50 buildings were declared landmarks and a facade improvement program was started. This program provided *low-cost loans and tax credits to building owners for improvements*. The Trust and the city commissioned a comprehensive master plan for the District. In the past 10 years plan goals that have been met include additional performance spaces, galleries, a new river front park, public art projects, and new shops and restaurants. District projects to-date have generated \$65 million in public investment, \$112 million in philanthropic support, and \$650 million in private investment. (Source: “Reinvigorating Cities: Smart Growth and Choices for Change”, April 19, 2000, National Building Museum).

- ❑ **TAX CREDITS, REGULATORY RELIEF AND TAX INCENTIVES - Lower Downtown (LoDo), Denver, Colorado** ■
 Denver's warehouse center in the late 19th and 20th century is the heart of a trendy, walkable neighborhood of loft apartments, galleries, and restaurants. Many LoDo buildings which were intended to be saved were actually torn down in the 1970s after property was rezoned from industrial to mixed use. Rezoning was intended to encourage reinvestment. Renewed efforts for change and cooperation across interests led to the rebirth of LoDo in the 1980s and 1990s. The 1986 Downtown Plan, the 1987 Urban Design Plan for Lower Downtown, and the 1991 Lower Downtown Streetscape Design Guidelines clarified a vision for the area as a walkable, mixed-use residential neighborhood. Additional changes to the zoning code and the creation of the Lower Downtown Historic District in 1988 enforced the vision. Building façade improvements and building code upgrades were supported by a *loan program* funded by the city, local businesses, preservation organizations, and the National Trust for Historic Preservation. *State and federal tax credits* also encouraged conversion of historic properties into lofts.
- ❑ **TAX ABATEMENT - Proposal Could Firm Schedule for Abatements** - Cleveland mayor has proposed legislation that would grant a 15 year, 75 *percent property tax abatement* on construction of all apartment buildings in downtown Cleveland. (Source: ULI web site, 06/01/2001).
- ❑ **REGULATORY RELIEF - Award-Winning Zoning Program Trades Building Height for Amenities, Housing** - Portland, Oregon *adopts zoning* that lets developers construct taller office buildings if project adds certain amenities or includes housing projects near public transportation. Adopted for a specific area of Portland. (Source: "Community Development Digest", January 6, 2001).
- ❑ **SPEEDIER PERMITTING, TAX INCENTIVES AND LAND SUBSIDIES - Smart Growth Model Based on Market Demand** - A smart growth model developed by Common Wealth Partners is based on 20 years of research on sprawl and its solutions. There are some barriers to smart growth (including infill development) in urban areas--such as legislation and a lack of incentives for developers. To solve this, it is recommended that local governments must provide incentives such as *land subsidies, speedier permit approvals, and tax rebates*. (Source: ULI web site, 06/01/2001).
- ❑ **GENERAL REFERENCE TO INCENTIVES AND PUBLIC-PRIVATE PARTNERSHIPS - Sustaining Local Communities** - The National Association of Homebuilders (NAHB) and the National Association of Counties (NACo) have agreed to work together to create sustainable local communities throughout the country. Both groups say they have embraced "smart growth" models and are looking to educate and inform their members, legislators, and the public about the importance of reforms that encourage productive land use and comprehensive planning. Each also agrees that creating a strong public-private partnership at the local level will best address sprawl-related problems like traffic congestion, crowded schools, and

the loss of open space. *"NACo and NAHB strongly believe affordable housing and smart growth issues require immediate action with locally derived solutions,"* said NACo President Jane Hague. "We will be working together to assure that quality housing for all Americans is an integral component of community planning." (Source: ULI web site, 06/01/2001).

EXAMPLES OF INFILL DEVELOPMENT INCENTIVES IN U.S. CITIES											
COMMUNITY, AGENCY OR ORGANIZATION NAME	STRATEGY OR TECHNIQUE SUGGESTED &/OR USED										
	Non-specific Incentives	Tax Incentives or Abatements	Faster Permit Approvals	Public-Private Partnership	Special Tax District	Housing or Density Bonuses	Transfer-able Development Rights	Fee Waivers	Regulatory Relief/ Special Regulations/ Inclusive Zoning	Home-ownership Incentives	Financial Incentives (Grants &/or Loans)
Austin, Texas											
Bradenton, Florida											
Cleveland, Ohio											
Dallas, Texas											
Denver, Colorado											
Green County, Maryland											
Kansas City, Missouri											
Pittsburgh, Pennsylvania											
Portland, Oregon											
Redmond, Washington											
South New Jersey towns											
U.S. Department of Agriculture											
U.S. House of Representatives											
State of California											

EXAMPLES OF INFILL DEVELOPMENT INCENTIVES IN U.S. CITIES											
COMMUNITY, AGENCY OR ORGANIZATION NAME	STRATEGY OR TECHNIQUE SUGGESTED &/OR USED										
	Non-specific Incentives	Tax Incentives or Abatements	Faster Permit Approvals	Public-Private Partnership	Special Tax District	Housing or Density Bonuses	Transfer-able Development Rights	Fee Waivers	Regulatory Relief/ Special Regulations/ Inclusive Zoning	Home-ownership Incentives	Financial Incentives (Grants &/or Loans)
Legislature											
Common Wealth Partners											
National Association of Homebuilders											
Urban Land Institute											

EXAMPLES OF INFILL/SMART GROWTH DEVELOPMENT PROJECTS IN U.S. CITIES

The following are examples of infill development or smart growth projects that have been reported in various locals throughout the United States.

- ❑ **Neal & Loia Loft-Style Multifamily Project, Atlanta Georgia:** A new, ground-up loft-style project built in the style of a reused factory structure. Market was determined as under-fed and the key ingredients identified of what consumers like about loft living. These elements included high ceilings, exposed utilities, an open floor plan, numerous windows, and lots of light. (Source: ULI web site, 06/01/2001).
- ❑ **Seventh and Collins Parking Garage, Miami Beach, Florida :** A public/private mixed use development featuring unique architectural details, a six-level garage, and street-level retail space has been built in the historic “art deco” South Beach neighborhood of Miami Beach. Project features high-end, street-level retail space, design incorporating native vegetation, preservation of historic facades, and a public/private partnership. (Source: ULI web site, 06/01/2001).
- ❑ **Bass Lofts, Atlanta Georgia ■** A 1920s-era high school and gymnasium was converted to 103 loft apartments and the development of 30 new units. Many of the school’s original features were retained in order to *obtain state historic preservation tax credits* and to offer unconventional living spaces to young, affluent professionals. Project features historic preservation tax credits, high-tech features. Gated parking, security elements, and new and historic units. (Source: ULI web

site, 06/01/2001).

- ❑ **The Cotton Mill, New Orleans, Louisiana** - Rehabilitation and conversion of a historic 323,000 s.f. cotton mill into 287 apartment and condominium units. 25,000 s.f. interior courtyard was preserved at the interior of 6 large multi-story structures that ringed a city block. Project features include historic preservation, adaptive reuse of key design elements and materials, on-site artwork, and redevelopment. (Source: ULI web site, 06/01/2001).
- ❑ **Peninsula Regent, San Mateo, California** - A 207-unit luxury high-rise continuing care retirement community was built on a two-acre site at the edge of downtown San Mateo. Project design included luxury environment and services of a first-class hotel while offering a comprehensive health care program that includes a wellness program, drop-in clinic, and services of an on-site medical director. Project features include one or two bedroom, customized residential units, on-site assisted living wing, landscaped recreational areas, gated and fenced facility, and equity preservation. (Source: ULI web site, 06/01/2001).
- ❑ **DePaul Center, Chicago, Illinois** - A historic department store in downtown Chicago was rehabilitated and converted to multiple use facility including: retail, office, and educational complex. Development considerations include its institutional-municipal partnership, rehabilitated urban landmark, and a innovative financial endeavor. Project features include: adaptive use and restoration of historic building, "correction" of architectural inconsistencies of original building, provision of two public spaces, innovative financial arrangement and "land-banking" space for expansion, consolidating vicinity as an urban campus.
- ❑ **60-Unit Detroit Housing Project** - New 60-unit infill housing project in Detroit. Homes built in groups of 11. Lease-to-own program for low to moderate income families. Federal tax credit program assisted project.
- ❑ **Victoria Townhomes, Seattle, Washington** - A turn-of-the-century apartment building was slated for rehabilitation and conversion to condominiums. Additional parking spaces (60) were constructed in a two-level partially underground structure. 10 new ground-related townhomes were constructed. Designed with input from neighborhood. Variances from zoning granted in setbacks, density, landscaping and bulk area requirements. New design review process initiated.
- ❑ **Portners Landing, Alexandria, Virginia** - Redevelopment of an old Civil War -era brewery into 20 townhome condominium units with underground parking and 38 apartment flats within a new 4-story condominium building adjacent the brewery with parking under the building. Twenty 20'-wide 4-story townhomes are built on the remainder of the site in a style reminiscent of the area's architectural legacy. Uses existing urban patterns, textures and details.
- ❑ **The Grand Treviso Apartments, Los Colinas, Texas** - A new mixed use project includes a 5-story parking structure with a pool complex and garden terrace on the roof of the parking structure. A 17-story, 247 unit structure will incorporate 12 residential floors above the garage. The parking structure is wrapped by a 4-story mid-rise structure which has parking on the same level. Individual utilities metering, washer and dryer connections, concierge service, fitness center and a clothes care facility are available onsite. The project has commercial bank financing.

EXAMPLES OF INFILL/SMART GROWTH DEVELOPMENT PROJECTS IN U.S. CITIES																			
PROJECT NAME & LOCATION	High Ceilings	Exposed Utilities	Open Floor Plan	Numerous Windows	Lots of Light	Street-Level Retail Space	Vegetation-Landscaping	Preserve Architectural-Historic Features	Public-Private Partnership	Tax Credits	Modern &/or High Tech Features	Structures &/or Gated Parking	Security Elements	New Construction	Existing Structure Reuse-Construction	Onsite Artwork	Comprehensive Health Care Program & Facilities	Assisted Living Wing	Equity Preservation
Neal & Loia Multi-Family Residential - Factory Style; Atlanta GA																			
Seventh & Collins Parking Garage; Miami Beach, FL																			
Bass Lofts (Residential); Atlanta GA																			
60-Unit Detroit Housing; Detroit, MI																			
Victoria Townhomes (Residential); Seattle, WA																			
Portners Landing (Res.); Alexandria, VA																			
The Grand Treviso Apartments; Los Colinas, TX																			
The Cotton Mill (Residential); New Orleans, LA																			
Peninsula Regent (Residential); San Mateo, CA																			

SOURCE: Planning and Economic Development Division, Urban Development Department, City of Tulsa, June 21, 2001.

APPENDIX I - EXAMPLES OF STREET FURNITURE AND LANDSCAPING

EXHIBIT I-1 - Street furniture including pedestrian-scale street lamps, newspaper stands, and crossing signals.



EXHIBIT I-2 - Street furniture and landscaping, including benches, street lamps, trash receptacles, landscaping and typical tree planting.





EXHIBIT I-3 - Pedestrian scale landscaping and brick planters and terraces, located at 17th Street and Quincy Avenue, St. John's Medical Center.



EXHIBIT I-4 - Pedestrian scale landscaping adjacent sidewalk and parking lot, located at 17th Street and Quincy Avenue, St. John's Medical Center.

APPENDIX J - EXAMPLES OF ■BUMP-OUTS, CURB EXTENSIONS AND/OR CROSSWALKS

EXHIBIT J-1 - ■Bump-outs• at crosswalk, Utica Square, Tulsa, Oklahoma.



EXHIBIT J-3 - Mid-block ■bump-out• and



EXHIBIT J-2 - ■Bump-out•, textured pedestrian crosswalk and streetscape elements, Scottsdale, AZ



curb extension in Utica Square,
Tulsa, Oklahoma.

EXHIBIT J-4 - Landscape with seasonal color at ■bump out• and crosswalk, Utica Square, Oklahoma.



EXHIBIT J-5 - Mid-block ■bump out• and crosswalk with landscaping, Utica Square, Tulsa, Oklahoma.

EXHIBIT J-6 - Third and Main Streets Tulsa, Oklahoma.



EXHIBIT J-7 - Third and Main Streets, Tulsa, Oklahoma.



APPENDIX K - ACKNOWLEDGMENTS

All property owners are considered members of the Brady Village Infill Neighborhood Task Force and T.I.F. District No. 1. All property owners listed in the Tulsa County Courthouse records were notified of Brady Village Task Force and T.I.F. No. 1 meetings Notification was made via property owner addresses provided in the Tulsa County Assessors Land Files Records. In addition, business tenants were surveyed and those expressing in interest receiving notification of meetings were notified. Additionally, notification of meetings and Brady Village Infill Task Force activities was provided to other expressing interest in Brady Village Infill Task Force efforts, including District 4 City Councilors, the Mayors Office, Oklahoma State University – Tulsa Campus, and appropriate municipal staff. Notice was provided via postal mail, email notice and posting of meeting date, time, location and agenda items at the offices of the Tulsa Urban Development Department 111 S. Greenwood Avenue, Tulsa Oklahoma, 74120.

Brady Village Infill Neighborhood Task Force and T.I.F. District No. 1 Members, Brady Village Owners’ Association Members, Area Property Owners and/or Representatives and Contacts, Interested Parties and Other Participants are set forth below:

307 Brady LLC	Beebe, Gary Dale	Cameron, David; Brady Village Owners’ Association Legal Counsel
Affordable Bail Bonds	Born Engineering Co.	Childers, Steve; Downtown Tulsa Unlimited
Baker, Tom, Tulsa City Council District 4	Brady Theater Corporation	Chromium Plating Co.
Bank of Oklahoma Corp	Bretanus, John	Clement, Tim
Bauer, Doug; Bauer Real Estate	Briggs, Larry D. & Terry L.	Conner, Don; Mexicali Border Cafe
Beard, David Earl & Onit	Building on Brady LLC	CRO, Herb Goodman; Spaghetti Warehouse

BRADY VILLAGE INFILL NEIGHBORHOOD DETAILED IMPLEMENTATION PLAN

Crossland, Cheral, Office of the Tulsa City Council	Goldman, Steve	King, L. A. Corp.,
Davis, Leon & Elliott	Helm, Jay; American Residential Group	Lausen, Fred W. Trustee
Dreyfus Realty Co.	Hess, James D., ED.D., Vice President, O.S.U.	Lemley, Mike
Evans, Lee James	Horner, Eric D.; Texas Tamales	Letson, Richard; Curly's
Fleenor, Richard G.	Howard, Sean	Lombardi, Tony; Tulsa Development Authority Representative
Ganzert, David; John 3:16 Missions	INTERAK Corporation	Martin, John, Manufacturer Inc.
Ganzkow, Steve; American Residential Group	Jacobs, Jack R.	Mayo, Peter O., Brady Theater
Garcia, Bradley R.; Gypsy Coffee House	Jalex Industries Inc.	McCoy, Peggy
General Manufacturer Inc.	James, Sandy	Midget, Dwain, Office of the Mayor, City of Tulsa
Glass, Jim; North Elgin Teleport and PPG Building	Jones, W. Douglas; INTERAK Corporation and Tercero Corporation Representative	Miller, Brenda; Tulsa Development Authority Director
Goad, C.L. & Catherine M.	Kenby Oil Company	Murray, Lawrence P. &

BRADY VILLAGE INFILL NEIGHBORHOOD DETAILED IMPLEMENTATION PLAN

Myers, Chris	Saleen Properties	Sweatac Inc
Newblock, E. I.	Sanditen, Wilfred	Tabler, William Sr. & Jyme
Nixon, Harold E.	Scott, Danny & Nancy	Tercero Corp
North Elgin Teleport LLC	Shaeffer, Larry E.; Cain's Ballroom	Tidwell, Ben Jr. & Boyce L.
Norton, Jim; Executive Director; Downtown Tulsa Unlimited	Sharp, David P. & George S. & Patricia Ann	Tollison, Alice Naomi
Oliphant, Greg	Sharp, David P.	Watts, Gary, Former City Councilor District 4
Olsen, Jeff, OMNI Lighting	Sharp Brothers Investment, Inc.	Weese, Ron; Sand Springs Home Representative
Peterson, Douglas G. & Victoria D. Co Trustees	Sooner Hardwoods LLC	Wildwood Enterprises Inc
Douglas G. Peterson Tr. & Etal	Southern Foods Group LP	Wiles, Bill; Davis Brothers Corp. Representative
Ryan, Terry; Sand Springs Home Representative	Stewart, Frank; North Elgin Teleport	Wright, Jack
Sager, Michael; Sager Properties	Streetman, George; Meadowgold-Bordens	
	Sutter, Paul; Meadowgold-Bordens	

BRADY VILLAGE INFILL NEIGHBORHOOD DETAILED IMPLEMENTATION PLAN

All property owners within the Brady Village Study Area and Brady Village T.I.F. District No. 1 area are considered non-voting members of the Brady Village Owners' Association (B.V.O.A.). Those Brady Village Property Owners that paid annual dues to the B.V.O.A. were considered voting members of the B.V.O.A.. The 2001 and 2002 members of the B.V.O.A. include:

Glass, Jim; North Elgin Teleport LLC; President, B.V.O.A. 2002

Jones, W. Douglas; ; INTERAK Corporation and Tercero Corporation Representative; Past President, B.V.O.A. 2001

Wiles, Bill; Davis Brothers Companies

Ganzert, David; John 3:16 Mission

Streetman, George; Meadowgold-Bordens

Sharp, David; Property Owner

Miller, Brenda K., Director; Tulsa Development Authority

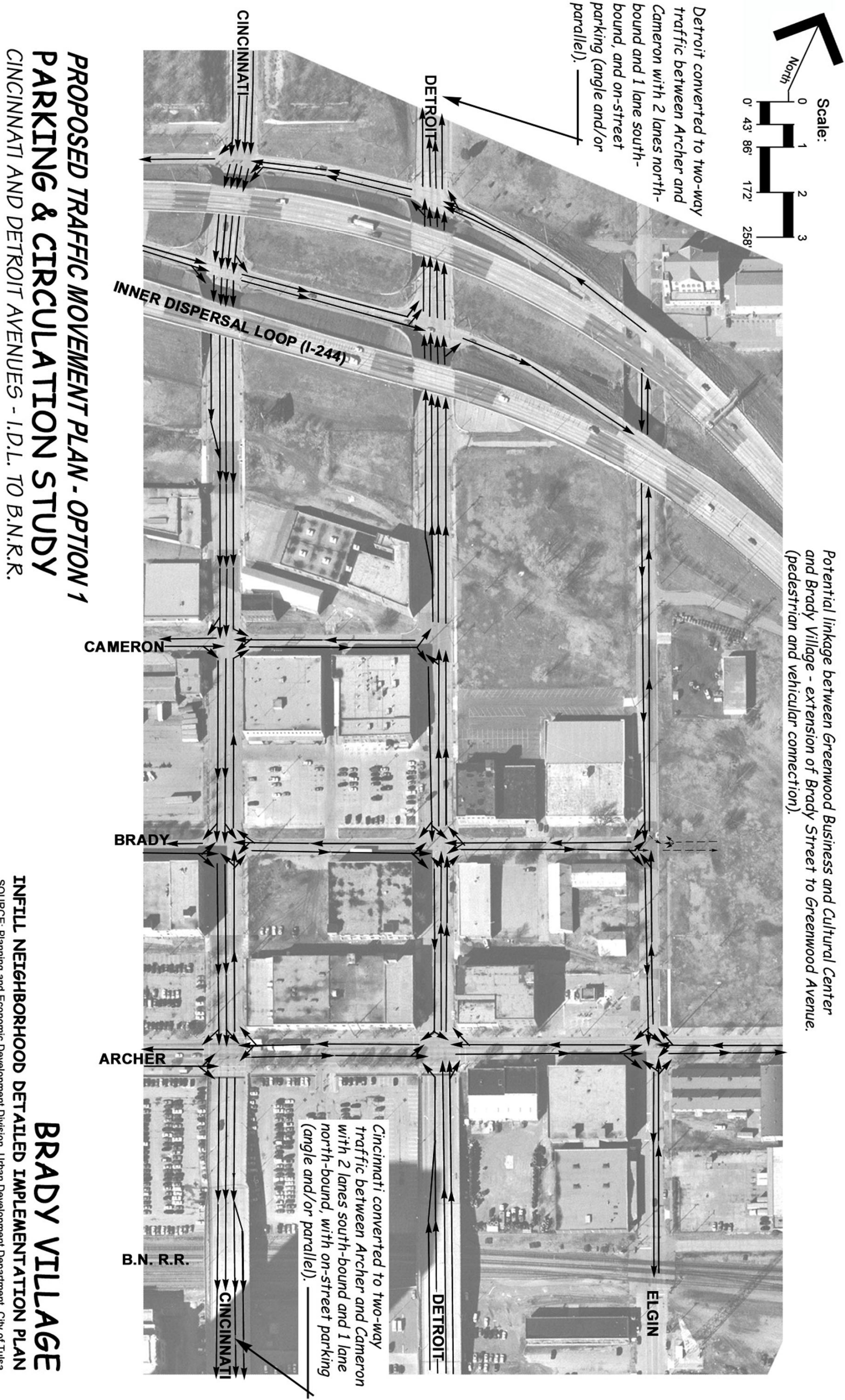
Weese, Ron; Sand Springs Home

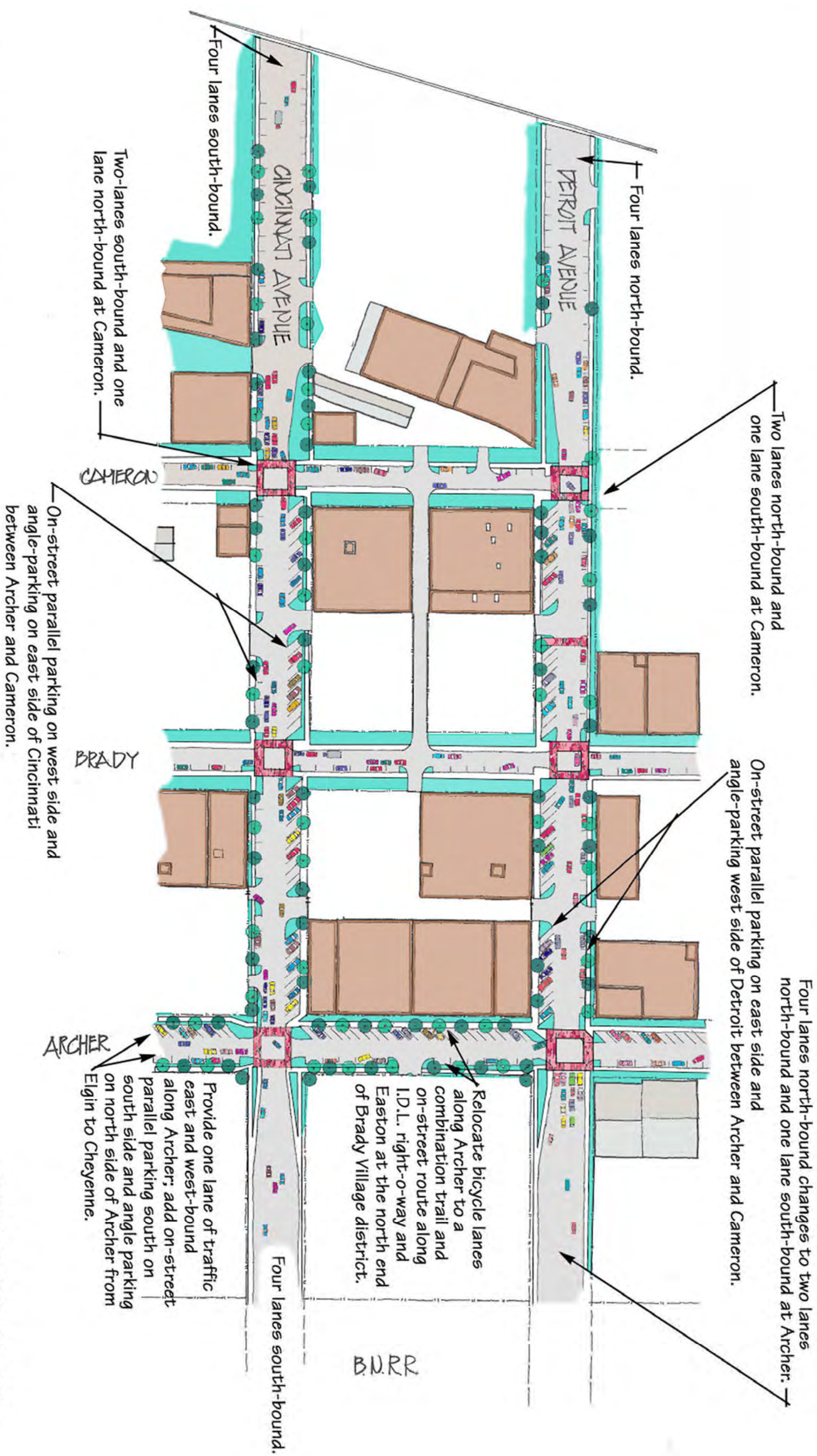
Ganzkow, Steve; American Residential Group

Norton, Jim; Executive Director (Non-voting member); Downtown Tulsa Unlimited

APPENDIX L

CIRCULATION STUDY OF CINCINNATI AND DETROIT AVENUES BETWEEN THE I.D.L. AND ARCHER STREETS, BRADY VILLAGE





PARKING & CIRCULATION STUDY

CINCINNATI AND DETROIT AVENUES - I.D.L. TO B.N.R.R.

CONVERTING CINCINNATI AND DETROIT AVENUES FROM ONE-WAY TO TWO-WAY TRAFFIC FROM ARCHER TO CAMERON STREETS

BRADY VILLAGE

INFILL NEIGHBORHOOD DETAILED IMPLEMENTATION PLAN

SOURCE: Planning and Economic Development Division, Urban Development Department, City of Tulsa

Scale:

0' 25' 50' 100' 150'

0 1 2 3

← north

APPENDIX M

EXAMPLES OF APPROPRIATE INFILL MIXED USE PARKING FACILITIES
Reprinted with permission of Walker Parking Consultants.

FORT COLLINS CIVIC CENTER
FORT COLLINS, COLORADO



Fort Collins, Colorado is a town of 110,000 in north central Colorado – home of the Colorado State University and county seat for Larimer County. It has a fairly compact downtown area undergoing significant growth. On the West Side of the downtown area several new governmental projects were planned, including a new Justice Center and City Office Building. In anticipation of the increased parking demand, and to further support the downtown retail parking demand, the city decided to construct a new 900 space parking structure on the southeast corner of Mason Street and LaPorte Avenue.

Fort Collins has a rich tradition of elegant masonry architecture. The client was concerned about the likely massiveness of a 900 space parking structure fitting comfortably into this context and desired a building which looked more like the charming downtown historical district rather than a bulky parking structure. The architect designed the façade that suggests a series of individual buildings constructed over time.

User amenities include 15,000 square feet of retail on the lower level, a dedicated pedestrian walkway on each level, a pedestrian connector bridge to the Open Galleria Building and the downtown business district, and an attractive “artist inspired” terrazzo system on the floor areas in front of the elevators.

The City of Fort Collins received a 2001 International Parking Institute Honorable Mention for the Civic Center Garage.



AUSTIN
3910 South IH35, Suite 300
Austin, TX 78704
512.373.4088

Walker has fifteen offices throughout the world to serve its clients efficiently and cost effectively.
Visit us on the World Wide Web at www.walkerparking.com

OLD TOWN WICHITA GARAGE

WICHITA, KANSAS



WALKER
PARKING CONSULTANTS



Facing a need for additional parking in downtown Wichita, the City selected the architectural firm of Law/Kingdon to design a new parking structure to just north of the Old Town Hotel. Walker Parking Consultants was selected to provide the functional design and parking consulting on the project. The structure provides parking for the public and hotel patrons.

Providing a design that was compatible with the surrounding warehouse buildings was a particular challenge. A predominantly brick façade with cut stone was selected for the project. Load bearing walls, arch window forms, and brick detailing were also used. Similar arch forms are found on some of the adjacent buildings. The repetition of projected steel balconies and ground level raised planters add to the architectural statement of the design. Custom shaped side beams were created to support the brick arch forms. The project both complements and enhances the architecture found in the old brick structures surrounding the site.

Walker's functional design provides three points of access and one point of egress for this four-level, 243-space garage.



AUSTIN
3910 South IH35, Suite 300
Austin, TX 78704
512.373.4088

Walker has fifteen offices throughout the world to serve its clients efficiently and cost effectively.
Visit us on the World Wide Web at www.walkerparking.com

APPENDIX N EXHIBIT N-1

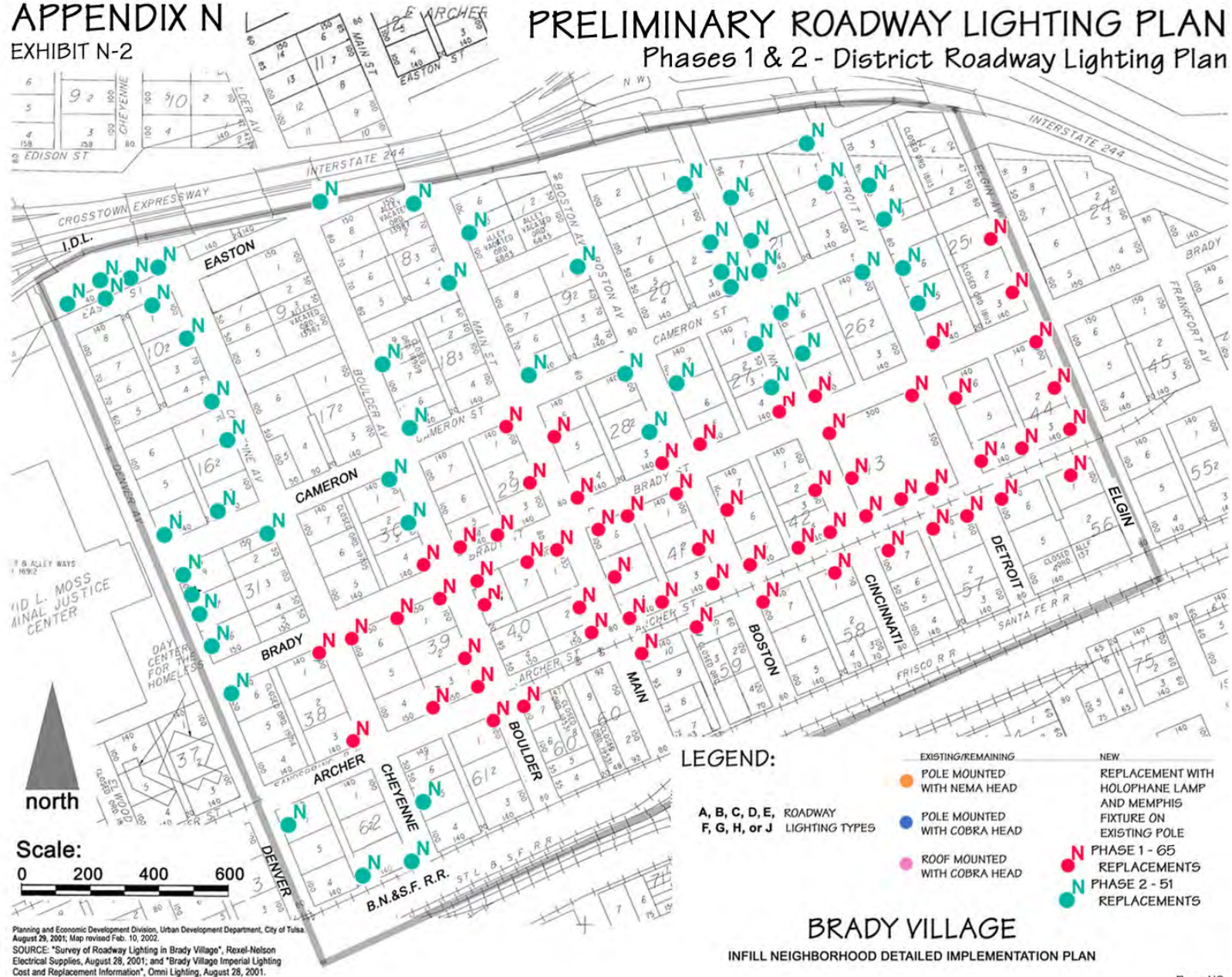
EXISTING ROADWAY LIGHTING District Lighting Plan



APPENDIX N
EXHIBIT N-2

PRELIMINARY ROADWAY LIGHTING PLAN

Phases 1 & 2 - District Roadway Lighting Plan



CONTACT:

Stephen D. Carr, A.I.C.P., Planner III
Planning and Economic Development Division
Urban Development Department
City of Tulsa
111 S. Greenwood Avenue, Suite 200
Tulsa, Oklahoma 74120

Telephone: (918) 596-2600
FAX: (918) 596-2608
EMAIL: SCarr@ci.tulsa.ok.us